



CONTRACT DRAWINGS FOR THE CONSTRUCTION OF

PAVEMENT REHABILITATION (APMS 1 & 2) TAXIWAY B4/C5

IMPERIAL COUNTY AIRPORT IMPERIAL, CALIFORNIA

AIP NO.: 3-06-0109-037-2019

C&S PROJECT: K30002019

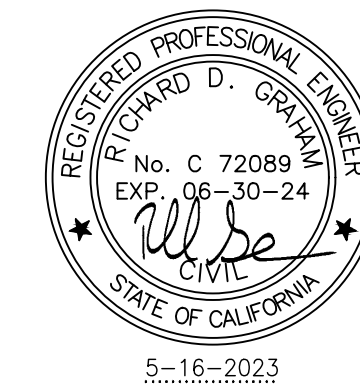
MAY 2023

DECLARATION OF RESPONSIBLE CHARGE

I HEREBY DECLARE THAT I AM THE ENGINEER OF WORK FOR THIS PROJECT, THAT I HAVE EXERCISED RESPONSIBLE CHARGE OVER THE DESIGN OF THE PROJECT AS DEFINED IN SECTION 6703 OF THE BUSINESS AND PROFESSIONS CODE THAT THE DESIGN IS CONSISTENT WITH CURRENT STANDARDS.

I UNDERSTAND THAT THE CHECK OF PROJECT DRAWINGS AND SPECIFICATIONS BY THE COUNTY OF IMPERIAL IS CONFINED TO A REVIEW ONLY AND DOES NOT RELIEVE ME, AS ENGINEER OF WORK, OF MY RESPONSIBILITIES FOR THE PROJECT DESIGN.

C&S COMPANIES
2355 NORTHSIDE DRIVE, SUITE 350
SAN DIEGO, CA 92108
(619) 296-9373



Richard Graham, P.E.
Engineer of Records
C 72089
06-30-24

Date

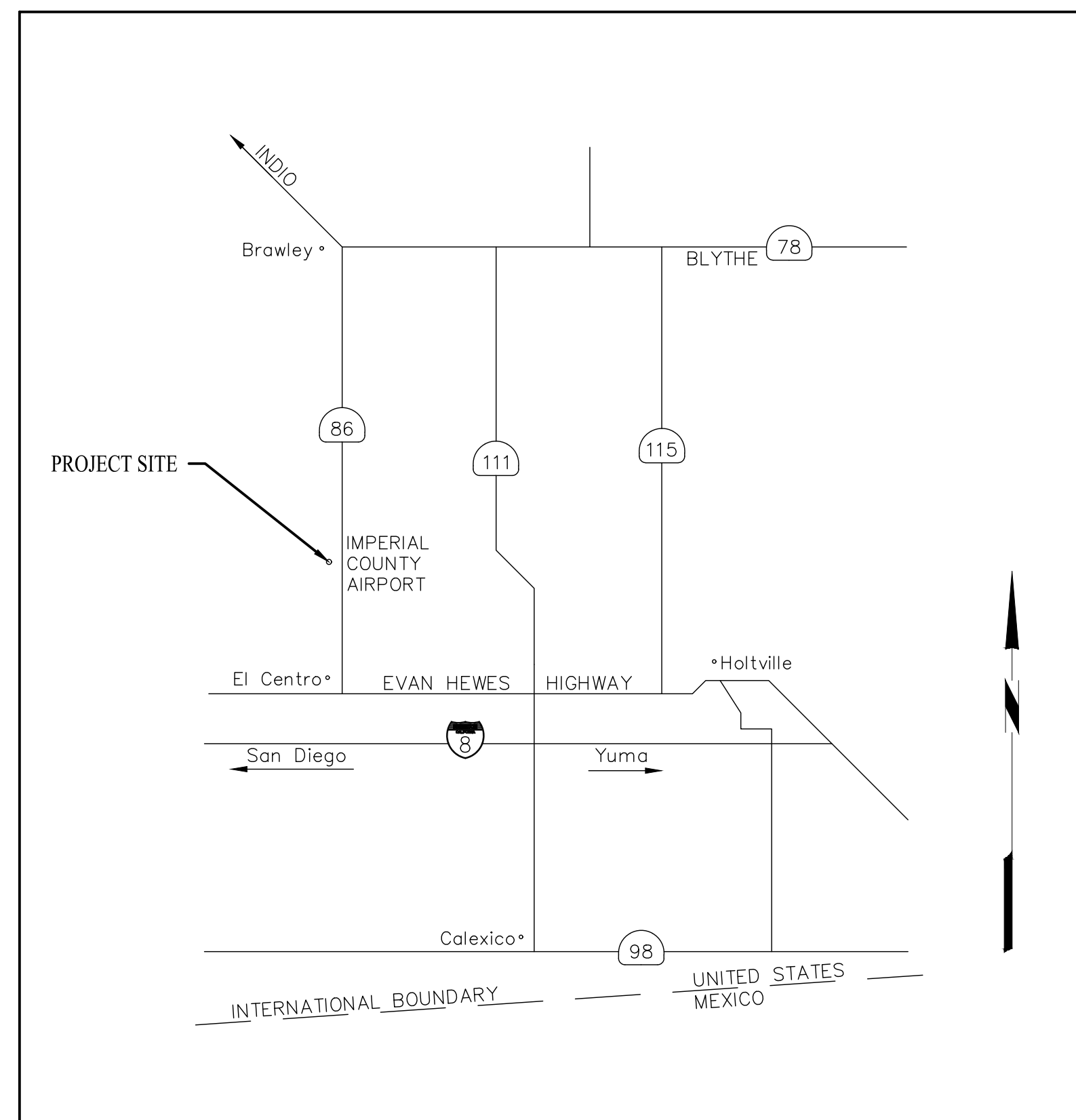
Underground Service Alert



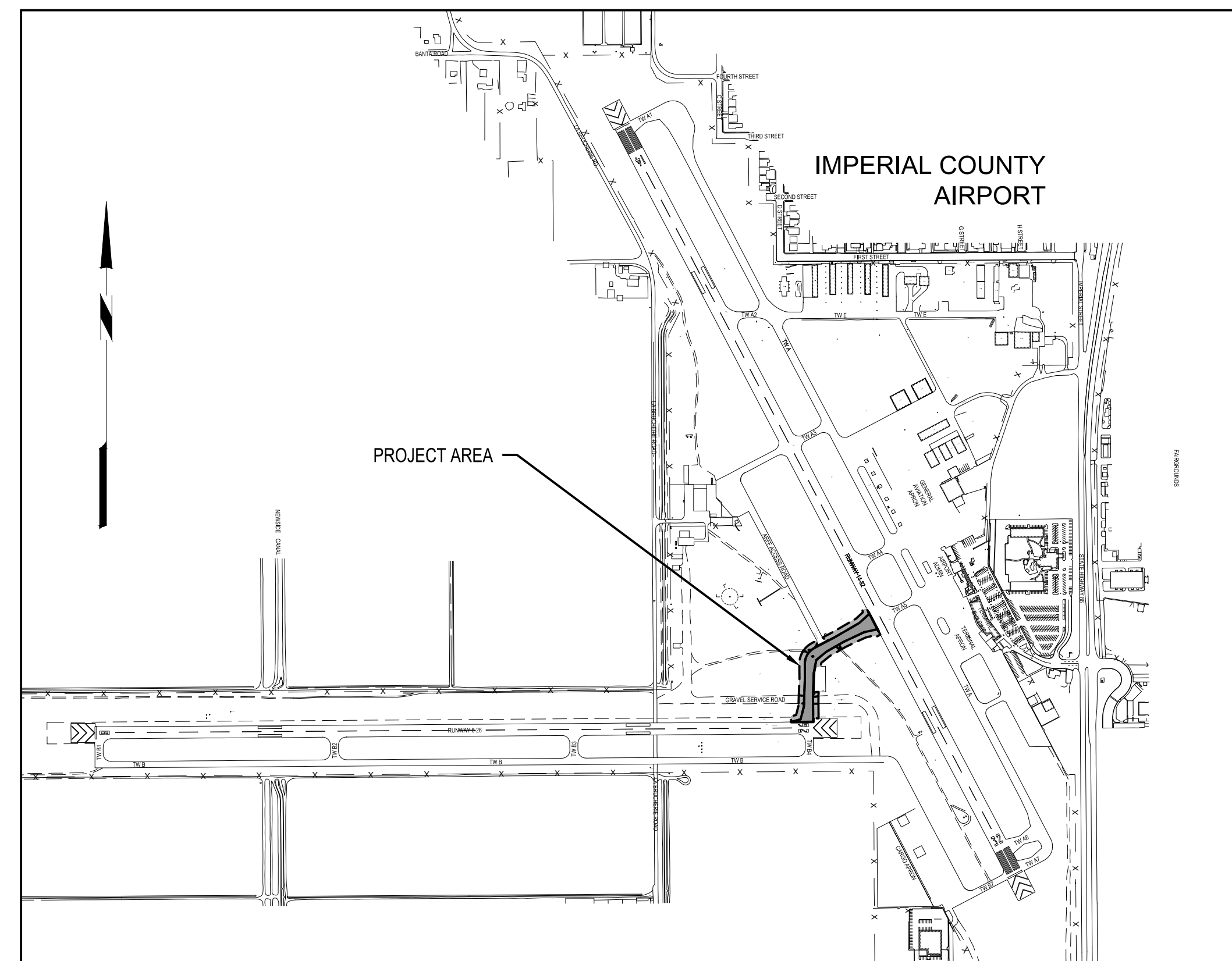
Call: TOLL FREE
"811"

TWO WORKING DAYS BEFORE YOU DIG

NO ALTERATION PERMITTED HEREON EXCEPT AS PROVIDED UNDER THE APPROVED REVISION PROCESS



VICINITY MAP
NOT TO SCALE



PROJECT LOCATION MAP
NOT TO SCALE

May 16, 2023 - 2:23PM
 F:\Project\K30 - Imperial County - Pavement Rehabilitation (Design)\CADD (Package 2)\Sheet Files\K30002019_G1-SERIES.DWG

SHEET	SHEET REFERENCE	TITLE
1	GI001	TITLE SHEET
2	GI002	SHEET INDEX & QUANTITIES FOR CANVASS OF BIDS
3	GI003	GENERAL NOTES
4	GI004	LEGEND, ABBREVIATIONS & SCHEDULE OF CIRCUITS
5	GC100	OVERALL CONSTRUCTION SAFETY PHASING PLAN
6	GC101	CONSTRUCTION SAFETY PHASING PLAN
7	GC501	CONSTRUCTION SAFETY PHASING DETAILS
8	CD101	DEMOLITION PLAN AND DETAILS
9	CS101	GEOMETRY PLAN
10	CG101	GRADING PLAN
11	CG201	PROFILE
12	CG202	CROSS SECTIONS
13	CG301	TYPICAL SECTIONS
14	CE101	SOIL EROSION AND SEDIMENT CONTROL PLAN
15	CE501	SOIL EROSION AND SEDIMENT CONTROL DETAILS
16	EL101	AIRFIELD LIGHTING AND SIGNAGE PLAN
17	EL501	AIRFIELD LIGHTING AND SIGNAGE DETAILS
18	EL502	AIRFIELD LIGHTING AND SIGNAGE DETAILS
19	XM101	MARKING PLAN
20	XM501	MARKING DETAILS
21	XM502	MARKING DETAILS

ITEM NO.	FAA SPEC	ITEM DESCRIPTION	QUANTITY	UNIT
1	C-101	CONTRACTOR QUALITY CONTROL PROGRAM (CQCP)	1	LS
2	C-102	INSTALLATION AND REMOVAL OF SILT FENCE	1,525	LF
3	C-102	INSTALLATION AND REMOVAL OF STORM DRAIN INLET PROTECTION	2	EA
4	C-102	4" CRUSHED AGGREGATE SHOULDER AND SLOPE PROTECTION	5,700	SY
5	C-105	MOBILIZATION (10% MAXIMUM)	1	LS
6	C-106	SAFETY, SECURITY, AND MAINTENANCE OF TRAFFIC	1	LS
7	P-101	AC PAVEMENT REMOVAL	4,900	SY
8	P-152	UNCLASSIFIED EXCAVATION	3,375	CY
9	P-154	STRUCTURAL GEOGRID REINFORCEMENT	6,000	SY
10	P-154	GEOTEXTILE FABRIC	6,000	SY
11	P-209	CRUSHED AGGREGATE BASE COURSE, 6" THICK	6,300	SY
12	P-219	RECYCLED CONCRETE AGGREGATE BASE COURSE	6,000	SY
13	P-401	BITUMINOUS SURFACE COURSE, GRADATION 2 (4" THICK)	1,420	TON
14	P-602	EMULSIFIED ASPHALT PRIME COAT	1,890	GAL
15	P-603	EMULSIFIED ASPHALT TACK COAT	445	GAL
16	P-620	MARKINGS	5,245	SF
17	P-620	OBLITERATE EXISTING PAVEMENT MARKINGS	1	LS
18	L-108	NO. 8 AWG, 5KV, 1/C AIRFIELD LIGHTING CABLE	2,490	LF
19	L-108	COUNTERPOISE WIRE, TRENCH AND BACKFILL	2,040	LF
20	L-110	2-INCH DIA. PVC CONDUIT	2,490	LF
21	L-110	REMOVE EXISTING ELECTRICAL CONDUIT	2,300	LF
22	L-115	EXISTING ELECTRICAL PULLBOX TO BE MODIFIED	2	EACH
23	L-125	REMOVE EXISTING TAXIWAY EDGE LIGHT, BASE MOUNTED	27	EACH
24	L-125	MEDIUM INTENSITY TAXIWAY EDGE LIGHT, BASE MOUNTED	39	EACH
25	L-125	REMOVE EXISTING LIGHTED GUIDANCE SIGN	8	EACH
26	L-125	RELOCATE EXISTING LIGHTED GUIDANCE SIGN	6	EACH
27	L-125	PROPOSED GUIDANCE SIGN	2	EACH

A1 SHEET INDEX
SCALE: NOT TO SCALE

A2 QUANTITIES FOR CANVASS OF BIDS
SCALE: NOT TO SCALE



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 www.cscos.com



IMPERIAL COUNTY AIRPORT
 PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: K30002019		
DATE: MAY 2023		
DRAWN BY: G.C. HAYDEN		
DESIGNED BY: M.E. BARR		
CHECKED BY: S.L. UNDERWOOD		

SHEET INDEX & QUANTITIES FOR CANVASS OF BIDS

GI002
2 of 21

May 16, 2023 - 2:23PM
F:\Project\K30 - Imperial County, CA\K30020219 - Pavement Rehabilitation (Design)\CADD (Package 2)\Sheet Files\K30020219_G1-SEPIRES.DWG

GENERAL CONSTRUCTION NOTES

1. THE CONTRACTOR'S ATTENTION IS DIRECTED TO SECTION 70-08, ATTACHMENT A - CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) OF THE GENERAL PROVISIONS.
2. THESE DRAWINGS HAVE BEEN PREPARED, IN PART, BASED UPON RECORD DRAWINGS AND/OR CAD FILES FURNISHED BY OTHERS. WHILE THIS INFORMATION IS BELIEVED TO BE RELIABLE, THOSE UTILIZING THE INFORMATION ON THESE DRAWINGS ARE ADVISED TO OBTAIN INDEPENDENT VERIFICATION OF ITS ACCURACY BEFORE USING IT FOR ANY PURPOSE.
3. EXISTING UTILITIES WERE TAKEN FROM PLANS OF RECORD. THEY HAVE BEEN SHOWN TO THE EXTENT KNOWN AND ARE OFFERED IN GOOD FAITH SOLELY FOR INFORMATIONAL PURPOSES. THEY MAY NOT REFLECT ACTUAL LOCATIONS AND MAY NOT BE INCLUSIVE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL UTILITIES PRIOR TO THE START OF CONSTRUCTION.
4. THE ACTUAL LOCATION AND ELEVATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.
5. IN THE EVENT OF DAMAGE TO EXISTING UTILITIES OR CABLES, THE ENGINEER AND OWNER SHALL BE NOTIFIED IMMEDIATELY.
6. THE CONTRACTOR SHALL REPAIR ALL DAMAGE TO UTILITIES OR CABLES, AS DIRECTED BY THE ENGINEER, IMMEDIATELY AND AT THE CONTRACTOR'S EXPENSE.
7. ALL AREAS DISTURBED AS A RESULT OF THE CONTRACTOR'S STAGING AND CONSTRUCTION OPERATIONS SHALL BE RESTORED EQUAL TO OR BETTER THAN ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
8. DURING THE WORK OF THIS CONTRACT, THE CONTRACTOR SHALL FURNISH, ERECT AND MAINTAIN WHATEVER TEMPORARY LIGHTING MAY BE NECESSARY TO KEEP THE TAXIWAYS IN OPERATING CONDITION WHEN OPEN FOR AIRCRAFT.
9. ALL DIRT, DUST, STONES AND LOOSE DEBRIS SHALL BE CONTINUOUSLY REMOVED FROM ALL PAVED SURFACES DURING THIS CONTRACT.
10. THE CONTRACTOR SHALL RECONSTRUCT AND MAINTAIN EXISTING ACCESS ROADS AS REQUIRED FOR ACCESS TO THE WORK AREAS.
11. THE CONTRACTOR SHALL CONSTRUCT AND MAINTAIN PROPOSED GRAVEL ACCESS ROADS AT THE APPROXIMATE LOCATION SHOWN.
12. PROPOSED ACCESS ROADS SHALL BE REMOVED UPON COMPLETION OF WORK AND THE AREA RESTORED TO ORIGINAL CONDITION.
13. ALL OF THE CONTRACTOR'S OPERATIONS SHALL REMAIN ON AIRPORT PROPERTY AT ALL TIMES. UNDER NO CIRCUMSTANCES WILL THE CONTRACTOR BE ALLOWED ON ADJACENT PROPERTY.
14. THIS CONTRACT DOES NOT ALLOW FOR PRICE INCREASES DUE TO ESCALATION IN COST OF UNIT BID ITEMS. THE CONTRACTOR SHALL TAKE THIS INTO CONSIDERATION WHEN PREPARING UNIT PRICES FOR BID.
15. THE COST OF ALL FAILING TESTS PERFORMED BY THE OWNER OR ON THE OWNER'S BEHALF SHALL BE BORNE BY THE CONTRACTOR.

GRADING AND EXCAVATION NOTES

16. SELECTIVE GRADING SHALL BE REQUIRED AS DIRECTED BY THE ENGINEER.
17. QUALITY ASSURANCE TESTS WILL BE MADE BY AND AT THE EXPENSE OF THE OWNER, UNLESS OTHERWISE NOTED. THE COST OF ALL FAILING TESTS SHALL BE BORNE BY THE CONTRACTOR.
18. THE EXACT LOCATIONS AND DIMENSIONS OF PAVEMENT TO BE RECONSTRUCTED SHALL BE DETERMINED BY THE ENGINEER DURING CONSTRUCTION.
19. ALL SPOIL SHALL BE DISPOSED OF OFF-SITE AT THE CONTRACTORS EXPENSE.
20. ALL SPOIL MATERIAL WHICH IS SUITABLE FOR EMBANKMENT SHALL BE DISPOSED OF ON AIRPORT PROPERTY AS SHOWN ON THE CONTRACT DRAWINGS, UNLESS OTHERWISE DIRECTED BY THE AIRPORT AUTHORITIES. SPOIL MATERIAL WHICH IS NOT SUITABLE FOR EMBANKMENT SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
21. EMBANKMENTS SHALL BE CONSTRUCTED WITH SUITABLE ON-SITE MATERIAL UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
22. ALL AREAS OUTSIDE OF THE GRADING LIMITS WHICH ARE DISTURBED SHALL BE RESTORED BY THE CONTRACTOR AT HIS EXPENSE.
23. THE COMBINATION OF SILT/CLAY SOILS AND HIGH NATURAL MOISTURE CONTENTS CREATE THE POTENTIAL FOR LOSS OF STRENGTH UNDER REPETITIVE LOADINGS OR VIBRATION. THE CONTRACTOR SHOULD TAKE THESE FACTORS INTO CONSIDERATION WHEN SELECTING EQUIPMENT, METHODS AND MEANS FOR CONSTRUCTION OF THIS PROJECT, AS WELL AS HAULING EQUIPMENT THAT WILL OPERATE IN THE AREA THROUGHOUT CONSTRUCTION. ANY DAMAGE TO THE SUBGRADE CONDITION AS A RESULT OF CONSTRUCTION OPERATIONS SHALL BE RESTORED TO EQUAL OR BETTER THAN ORIGINAL CONDITION, AS DIRECTED BY THE ENGINEER AND ALL AT THE CONTRACTOR'S EXPENSE.
24. TEMPORARY AIR AND WATER POLLUTION, SOIL EROSION AND SILTATION CONTROL WORK PERFORMED FOR PROTECTION OF CONSTRUCTION AREAS OUTSIDE THE CONSTRUCTION LIMITS, SUCH AS BORROW AREAS AND WASTE AREAS, HAUL ROADS, EQUIPMENT AND MATERIAL STORAGE SITES, AND TEMPORARY PLANT SITES, WILL NOT BE MEASURED AND PAID FOR DIRECTLY BUT SHALL BE CONSIDERED AS A SUBSIDIARY OBLIGATION OF THE CONTRACTOR.
25. ALL SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MATERIALS SHALL BE IN PLACE PRIOR TO BEGINNING EARTHWORK OPERATIONS AND SHALL BE MAINTAINED UNTIL THE

NEW SLOPES ARE STABILIZED WITH SEEDING AND/OR SLOPE PROTECTION.

SURVEY NOTES

26. FOR TYPICAL SECTIONS, THE CONTOUR INTERVAL EQUALS 1 FOOT. FOR TRANSITIONAL AREAS TO KEYWAYS, THE CONTOUR INTERVAL EQUALS 0.1 FOOT.
 27. ALL ELEVATIONS REFER TO NAVD 88 VERTICAL DATUM. COORDINATES REFER NAD 83 HORIZONTAL DATUM.
 28. THE TOPOGRAPHIC FEATURES SHOWN HEREON WERE COMPILED FROM FIELD SURVEY PERFORMED BY GUYETT GEOMATICS INC., DATED 7/15/20.
- PAVING NOTES**
29. ALL AREAS TO BE OVERLAID SHALL BE PREPARED IN ACCORDANCE WITH ITEM P-101, "PREPARATION/REMOVAL OF EXISTING PAVEMENTS".
 30. THE CONTRACTOR'S ATTENTION IS DIRECTED TO ITEM P-101 "PREPARATION/REMOVAL OF EXISTING PAVEMENTS" AS IT RELATES TO FILLING JOINTS AND CRACKS IN EXISTING PAVEMENT.
 31. EMULSIFIED ASPHALT TACK COAT, ITEM P-603, SHALL BE APPLIED PRIOR TO PLACING EACH LIFT OF PAVEMENT, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
 32. TRANSVERSE PAVING JOINTS IN ONE LAYER SHALL LINE UP WITH TRANSVERSE JOINTS IN THE PREVIOUS LAYERS UNLESS OTHERWISE APPROVED BY THE ENGINEER.
 33. TRANSVERSE PAVING JOINTS IN ADJACENT LANES SHALL LINE UP WITH EACH OTHER EXTENDING ACROSS THE FULL WIDTH OF PAVEMENT.
 34. IN CASES OTHER THAN CENTERLINE JOINTS, LONGITUDINAL PAVING JOINTS IN ONE LAYER SHALL BE OFFSET FROM THAT IN THE PREVIOUS LAYER BY AT LEAST ONE FOOT. THE JOINT AT THE CENTERLINE OF THE PAVEMENT SHALL LINE UP WITH PREVIOUS LAYER CENTERLINE JOINTS.
 35. COLD JOINTS SHALL BE SAWCUT BACK A MINIMUM OF 6 INCHES TO EXPOSE A CLEAN, SOUND, UNIFORM VERTICAL SURFACE FOR THE FULL DEPTH OF THE LIFT. THE SAWCUT SHALL NOT BE PERFORMED UNTIL THE PAVEMENT HAS REACHED AMBIENT TEMPERATURE.
 36. DELAMINATED PAVEMENT SHALL BE REMOVED BY COLD MILLING. THE LIMITS OF DELAMINATED PAVEMENT SHALL BE SAW CUT. THE LOCATION OF THE LIMITS OF DELAMINATED PAVEMENT WILL BE DETERMINED BY THE ENGINEER.

ELECTRICAL AND SIGNAGE NOTES

37. ALL ELECTRICAL WORK SHALL CONFORM TO APPLICABLE LOCAL, STATE AND NATIONAL ELECTRICAL CODES.
38. THE ELECTRICAL CHARACTERISTICS OF PROPOSED EQUIPMENT SHALL BE VERIFIED TO BE COMPATIBLE WITH EXISTING EQUIPMENT MANUFACTURER PRIOR TO INSTALLATION.
39. ABANDONED CABLES MAY EXIST IN THE VICINITY OF THE PROPOSED WORK. IF ENCOUNTERED, CONTRACTOR SHALL VERIFY THAT THEY ARE ABANDONED PRIOR TO REMOVAL. IF THEY ARE NOT ABANDONED, CABLES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
40. ITEMS OF SPECIFIC MANUFACTURE SHALL BE INSTALLED IN STRICT ACCORDANCE WITH MANUFACTURER'S PRINTED INSTRUCTIONS AND OR MANUFACTURER'S REPRESENTATIVE DIRECTIONS.
41. ALL GROUND CONNECTIONS SHALL BE MADE USING EXOTHERMIC CONNECTIONS.
42. GROUND RODS SHALL BE INSTALLED AT 500-FT INTERVALS ALONG COUNTERPOISE WIRE.
43. ALL CABLE CONNECTIONS SHALL BE MADE AT LIGHT UNITS OR AT ENDS OF DUCT BANKS UNLESS DIRECTED OTHERWISE.
44. THE OWNER RESERVES THE RIGHT TO SALVAGE LIGHTING EQUIPMENT. THE EQUIPMENT TO BE SALVAGED IS IDENTIFIED IN THE SPECIFICATION. SALVAGED EQUIPMENT SHALL BE STOCKPILED AT A LOCATION DESIGNATED BY THE OWNER IN PROPER WORKING CONDITION. ALL OTHER LIGHTING EQUIPMENT SHALL BE SPOILED OFF AIRPORT PROPERTY AT A PROPER DISPOSAL SITE SELECTED BY THE CONTRACTOR.
45. PROVIDE WATERTIGHT TERMINATION FOR ALL BURIED CONDUIT ENDS.
46. ALL RUNWAY AND TAXIWAY EDGE LIGHTS SHALL BE LOCATED 10 FEET OFF THE DEFINED PAVEMENT EDGE UNLESS OTHERWISE NOTED OR DIRECTED. THE CONTRACTOR SHALL ALIGN ALL LIGHTS ON TANGENT SECTIONS SUCH THAT THEY FORM A STRAIGHT LINE.
47. WHEN DETERMINING THE NUMBER OF CHARACTERS IN A GUIDANCE SIGN LEGEND THE CHARACTERS 1 , . (DOT) , - (DASH) WILL BE CONSIDERED ONE HALF CHARACTER. PAYMENT WILL BE FOR THE SUM OF ALL CHARACTERS ON THE LONGEST FACE ROUNDED UP TO THE WHOLE NUMBER. CHARACTERS ON THE OPPOSITE SIDE OF THE SIGN WILL NOT BE COUNTED.
48. WHEN DETERMINING THE NUMBER OF CHARACTERS IN A GUIDANCE SIGN LEGEND THE SYMBOL ON THE OPPOSITE SIDE OF A RUNWAY HOLDING POSITION SIGN WHICH REPRESENTS THE RUNWAY SAFETY AREA OR THE ILS CRITICAL AREA WILL BE CONSIDERED 4 CHARACTERS.

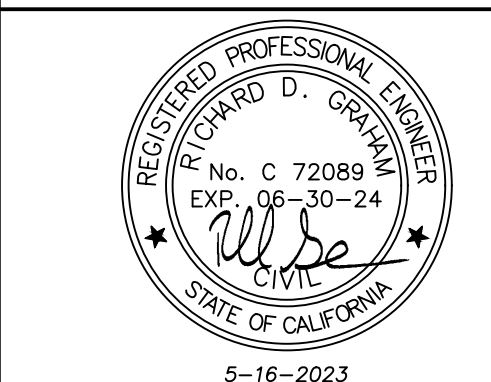
MARKING NOTES

49. REMOVAL OF EXISTING MARKINGS SHALL BE IN ACCORDANCE WITH ITEM P-101.

A1 GENERAL NOTES
SCALE: NOT TO SCALE



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IMPERIAL CALIFORNIA

 IMPERIAL COUNTY AIRPORT

 PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5

MARK	DATE	DESCRIPTION
REVISIONS		
	PROJECT NO:	K30020219
	DATE:	MAY 2023
	DRAWN BY:	G.C. HAYDEN
	DESIGNED BY:	M.E. BARR
	CHECKED BY:	S.L. UNDERWOOD

GENERAL NOTES

G1003
3 of 21

	KEYED NOTE REFERENCE		PROPOSED UNDERGROUND ELECTRIC LINE
	EXISTING AVIGATION EASEMENT BOUNDARY		PROPOSED BASE MOUNTED EDGE LIGHT
	EXISTING CONTOUR LINE		PROPOSED IN-PAVEMENT EDGE LIGHT
	EXISTING SWALE CENTERLINE		PROPOSED RETROREFLECTIVE MARKER
	EXISTING TOP/BOTTOM OF DITCH		PROPOSED AIRFIELD GUIDANCE SIGN
	EXISTING DRAINAGE LINE		PROPOSED SIGN UNIT ID TAG NUMBER
	EXISTING UNDERDRAIN		PROPOSED REIL UNIT
	EXISTING UNDERDRAIN CLEANOUT		PROPOSED ELECTRICAL DISCONNECT
	EXISTING CATCH BASIN		PROPOSED TRANSFORMER
	EXISTING DRAINAGE MANHOLE		PROPOSED ELECTRICAL MANHOLE
	EXISTING HEADWALL		PROPOSED PULLBOX
	EXISTING PIPE END SECTION		PROPOSED JUNCTION CAN
	EXISTING WATER LINE		PROPOSED AIRFIELD LIGHTING CABLE IN CONDUIT WITH CIRCUIT NUMBER AND NUMBER OF CABLES
	EXISTING UNDERGROUND TELEPHONE LINE		PROPOSED SILT FENCE LOCATION
	EXISTING UNDERGROUND ELECTRIC LINE		PROPOSED STORM DRAIN INLET PROTECTION
	EXISTING CONCRETE MARKER		PROPOSED HMA PAVEMENT
	EXISTING CONCRETE DUCT MARKER		PROPOSED HMA SHOULDER PAVEMENT
	EXISTING CONCRETE CABLE MARKER		PROPOSED PCC PAVEMENT
	EXISTING CONCRETE SPLICE MARKER		EXISTING PAVEMENT TO BE REMOVED
	EXISTING BOLLARD OR POST		FULL DEPTH EXCAVATION
	EXISTING TIE-DOWN		BARRICADE LOCATION WITH WORK AREA DESIGNATION
	EXISTING SINGLE POST TRAFFIC SIGN		WORK AREA LIMITS
	EXISTING DOUBLE POST TRAFFIC SIGN		FLAGPERSON LOCATION
	EXISTING MONITORING WELL		
	EXISTING OBSERVATION WELL		
	EXISTING HOLDING POSITION LIGHT		
	EXISTING SURFACE SENSOR		
	EXISTING RETROREFLECTIVE MARKER		
	EXISTING RUNWAY OR TAXIWAY EDGE LIGHT		
	EXISTING AIRFIELD GUIDANCE SIGN		
	EXISTING REIL UNIT		
	EXISTING ELECTRICAL DISCONNECT		
	EXISTING PULLBOX		
	EXISTING CIRCUIT LABEL		
	EXISTING AIRFIELD LIGHTING CABLE IN CONDUIT		
	EXISTING PULLBOX TO BE MODIFIED		
	EXISTING DRAINAGE LINE TO BE REMOVED		
	EXISTING CATCH BASIN TO BE REMOVED		
	EXISTING UNDERGROUND TELEPHONE LINE TO BE REMOVED		
	EXISTING TELEPHONE JUNCTION BOX TO BE REMOVED		
	EXISTING TELEPHONE MANHOLE TO BE REMOVED		
	EXISTING UNDERGROUND ELECTRIC LINE TO BE REMOVED		
	EXISTING RETROREFLECTIVE MARKER TO BE REMOVED		
	EXISTING RUNWAY OR TAXIWAY EDGE LIGHT TO BE REMOVED		
	EXISTING AIRFIELD GUIDANCE SIGN TO BE REMOVED		
	EXISTING PULLBOX TO BE REMOVED		
	EXISTING AIRFIELD LIGHTING CABLE IN CONDUIT TO BE REMOVED		
	EXISTING MARKING TO BE REMOVED		
	PROPOSED CONTOUR LINE		
	PROPOSED INTERMEDIATE CONTOUR LINE		
	PROPOSED SPOT ELEVATION		
	PROPOSED GRADE LINE		
	PROPOSED DRAINAGE LINE		
	PROPOSED UNDERDRAIN		
	PROPOSED UNDERDRAIN CLEANOUT		
	PROPOSED CATCH BASIN		
	PROPOSED DRAINAGE MANHOLE		
	PROPOSED PIPE END SECTION		
	PROPOSED WATER LINE		
	PROPOSED HYDRANT		
	PROPOSED WATER VALVE		

ABAN - ABANDONED APPROX - APPROXIMATE ASPH - ASPHALT B - BASELINE BLDG - BUILDING BM - BENCH MARK BUR - BURIED C - CENTERLINE CIP - CAST IRON PIPE CMP - CORRUGATED METAL PIPE CONC - CONCRETE CPVC - CORRUGATED POLYVINYL CHLORIDE PIPE CSP - CORRUGATED STEEL PIPE DIA - DIAMETER ELEV - ELEVATION FND - FOUNDATION HP - HIGH POINT INV - INVERT LT - LEFT LP - LOW POINT MAX - MAXIMUM MIN - MINIMUM MISC - MISCELLANEOUS NA - NOT APPLICABLE OFA - OBJECT FREE AREA O.C - ON CENTER PAVT - PAVEMENT PC - POINT OF CURVATURE	PCC - PORTLAND CEMENT CONCRETE PAVEMENT PI - POINT OF INTERSECTION PL - PROPERTY LINE POB - POINT OF BEGINNING POE - POINT OF ENDING PT - POINT OF TANGENCY PVI - POINT OF VERTICAL INTERSECTION PVC - POINT OF CURVATURE (VERTICAL CURVE) PVC - POLYVINYL CHLORIDE PIPE PPVC - PERFORATED POLYVINYL CHLORIDE PIPE PVT - POINT OF TANGENCY (VERTICAL CURVE) R - RADIUS ROD - RECORD RCP - REINFORCED CONCRETE PIPE ROFA - RUNWAY OBJECT FREE AREA RSA - RUNWAY SAFETY AREA ROW - RIGHT OF WAY RPZ - RUNWAY PROTECTION ZONE RT - RIGHT RW - RUNWAY SHDR - SHOULDER SICPP - SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE STA - STATION TOFA - TAXIWAY OBJECT FREE AREA TSA - TAXIWAY SAFETY AREA TW - TAXIWAY TYP - TYPICAL UD - UNDERDRAIN
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C4 ABBREVIATIONS
 SCALE: NOT TO SCALE

RUNWAY 14-32			TAXIWAY A		
STATION	NORTHING	EASTING	STATION	NORTHING	EASTING
POB: 0+00	1,887,430.73	6,767,437.31	POB: 300+00	1,887,558.48	6,767,680.84
POE: 63+00	1,881,851.73	6,770,363.88	POE: 363+00	1,881,979.48	6,770,607.40
LENGTH: 6300'			LENGTH: 6300'		
TAXIWAY A3			RUNWAY 8-26		
STATION	NORTHING	EASTING	STATION	NORTHING	EASTING
POB: 120+00	1,884,748.66	6,768,279.62	POB: 400+00	1,883,176.47	6,763,867.53
POE: 128+00	1,885,120.29	6,768,988.07	POE: 455+00	1,883,214.70	6,769,367.13
LENGTH: 800'			LENGTH: 5500'		
TAXILANE 1			TAXIWAY B4 - C5		
STATION	NORTHING	EASTING	STATION	NORTHING	EASTING
POB: 130+00	1,884,847.63	6,769,074.63	POB: 531+00	1,884,108.99	6,769,684.77
POE: 132+00	1,884,940.54	6,769,251.74	PT: 540+09.27	1,883,686.60	6,768,879.55
LENGTH: 200'			Δ: 62° 43' 05.52" R R: 75' PC: 540+91.37 1,883,619.67 6,768,839.40 POE: 547+00 1,883,011.05 6,768,843.63 LENGTH: 1600'		

B4 HORIZONTAL ALIGNMENT
 SCALE: NOT TO SCALE

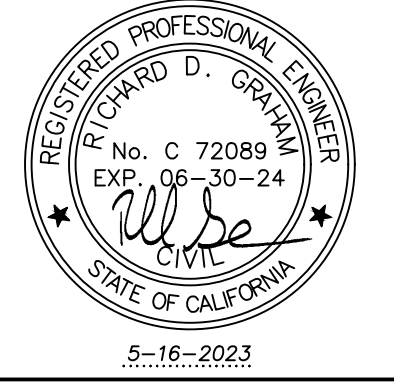
TAXIWAY A3 AND RUNWAY 14-32	125+00.00'	=	27+66.40'
TAXIWAY A AND TAXIWAY A3	327+66.40'	=	127+75.00'
TAXIWAY A AND TAXILANE 1	330+48.07'	=	130+25.00'
TAXIWAY A AND TAXIWAY C5	339+85.60'	=	533+27.82'
TAXIWAY C5 AND RUNWAY 14-32	536+52.82'	=	39+85.60'
TAXIWAY B4 AND RUNWAY 8-26	545+00.00'	=	449+74.82'

A1 LEGEND
 SCALE: NOT TO SCALE

A4 STATION EQUALIZATION
 SCALE: NOT TO SCALE



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IMPERIAL COUNTY AIRPORT
 PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5

MARK	DATE	DESCRIPTION
REVISIONS		

LEGEND, ABBREVIATIONS & SCHEDULE OF CIRCUITS

G1004
 4 of 21

BASIS OF BEARINGS

THE BASIS OF BEARINGS FOR THIS SURVEY IS BASED ON THE CALIFORNIA COORDINATE SYSTEM, ZONE 6 NAD 83 (EPOCH 2010) AS DETERMINED LOCALLY BY A LINE BETWEEN NGS POINT AA6905 AND POINT AA6906 BEING N82°25'42"W AS DERIVED FROM GEODETIC VALUES PUBLISHED BY NATIONAL GEODETIC SURVEY (NGS), RESPECTIVELY.

POINT AA6905 POINT AA6906
 N=1882962.41 N=1883311.96
 E=6767847.12 E=6765217.47

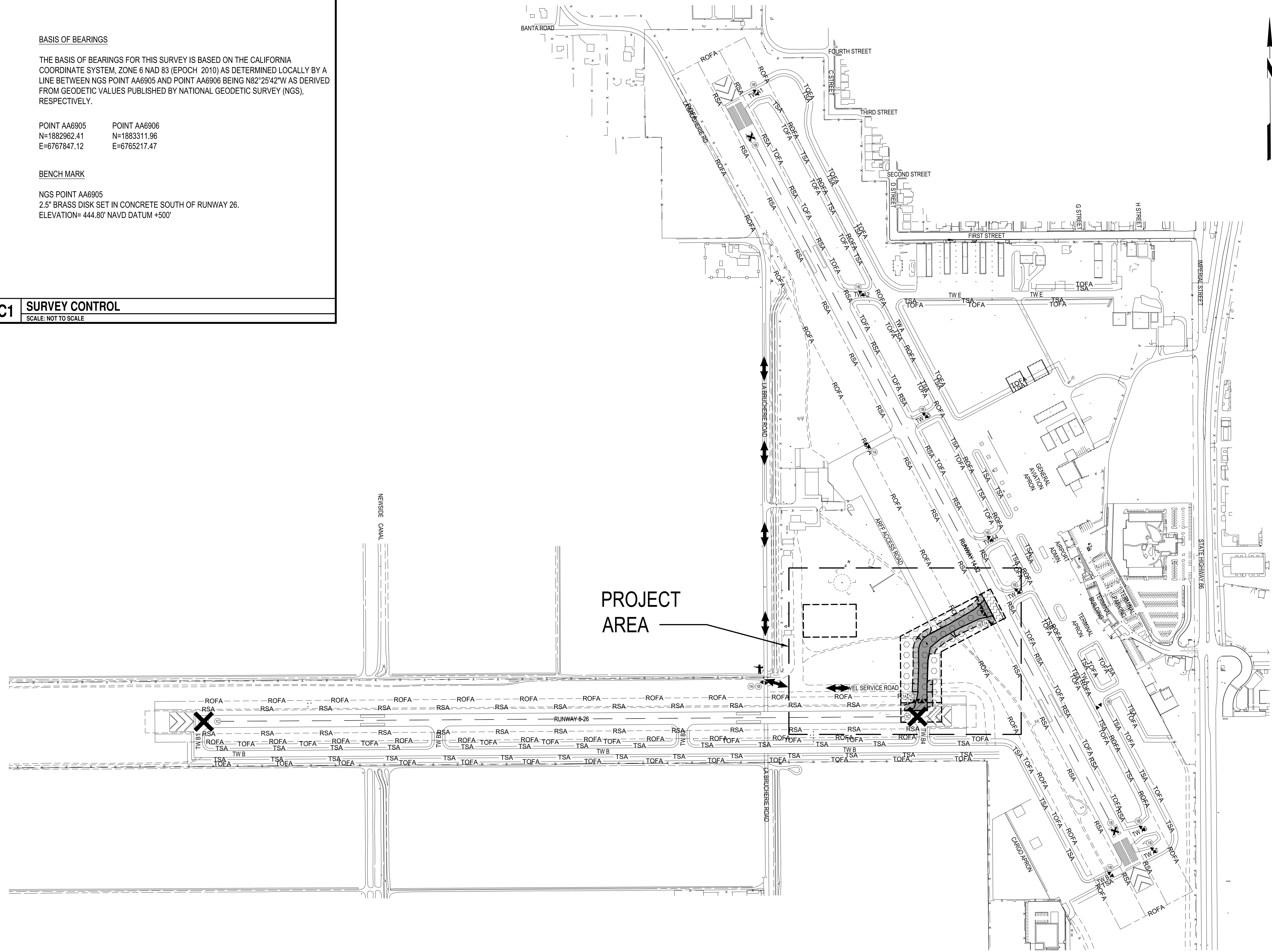
BENCH MARK

NGS POINT AA6905
 2.5" BRASS DISK SET IN CONCRETE SOUTH OF RUNWAY 26.
 ELEVATION= 444.80' NAVD DATUM +500'

C1 SURVEY CONTROL
 SCALE: NOT TO SCALE

B

A



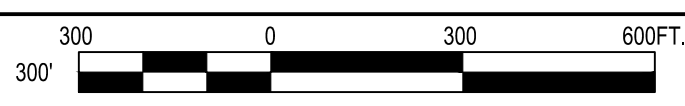
FAIRGROUNDS

IMPERIAL COUNTY AIRPORT
**PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5**

MARK	DATE	DESCRIPTION
REVISIONS		
		PROJECT NO: K30002019
		DATE: MAY 2023
		DRAWN BY: G.C. HAYDEN
		DESIGNED BY: M.E. BARR
		CHECKED BY: S.L. UNDERWOOD

**OVERALL
 CONSTRUCTION
 SAFETY PHASING PLAN**

GC100
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A1 OVERALL CONSTRUCTION SAFETY PHASING PLAN
 SCALE: 1" = 300'

May 4, 2023 - 12:48PM
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PHASING NOTES:

WORK AREA 1A NOTES:

- WORK IN THIS AREA INCLUDES WORK IN TAXIWAY B4/C5 NORTH OF RUNWAY 8-26 RSA AND WEST OF RUNWAY 14-32 RSA.
- WORK IN THIS AREA SHALL BEGIN FIRST.
- WORK IN THIS AREA CAN BE CONCURRENT WITH WORK IN WORK AREAS 1B AND 1C.

WORK AREA 1B NOTES:

- WORK IN THIS AREA INCLUDES WORK WITH IN THE RSA OF RUNWAY 14-32.
- WORK IN THIS ARE SHALL BE LIMITED TO A MAXIMUM OF 30 NIGHTS WITH WORKING HOURS RESTRICTED TO 8:00PM TO 5:00AM.
- WORK IN THIS AREA CAN BE CONCURRENT WITH WORK IN WORK AREA 1A, BUT MAY NOT BE CONCURRENT WITH WORK IN WORK AREA 1C.
- AT THE COMPLETION OF EACH WORK DAY, THE WORK AREA SHALL BE REOPENED TO ALLOW FOR RUNWAY 14-32 TO BE REOPENED FOR AIRCRAFT TRAFFIC INCLUDING PLACING TEMPORARY RAMPS TO ELIMINATE ANY PAVEMENT DROPS GREATER THAN 3 INCHES.

WORK AREA 1C NOTES:

- WORK IN THIS AREA INCLUDES WORK WITHIN THE RSA OF RUNWAY 8-26.
- WORK IN THIS AREA SHALL BE LIMITED TO A MAXIMUM OF 30 NIGHTS WITH WORKING HOURS RESTRICTED TO 8:00PM TO 5:00AM.
- WORK IN THIS AREA CAN BE CONCURRENT WITH WORK IN WORK AREA 1A BUT NOT CONCURRENT WITH WORK IN WORK AREA 1B.
- AT THE COMPLETION OF EACH WORK DAY, THE WORK AREA SHALL BE REOPENED TO ALLOW FOR RUNWAY 8-26 TO BE REOPENED FOR AIRCRAFT TRAFFIC INCLUDING PLACING TEMPORARY RAMPS TO ELIMINATE ANY PAVEMENT DROPS GREATER THAT 3 INCHES.

GENERAL NOTES:

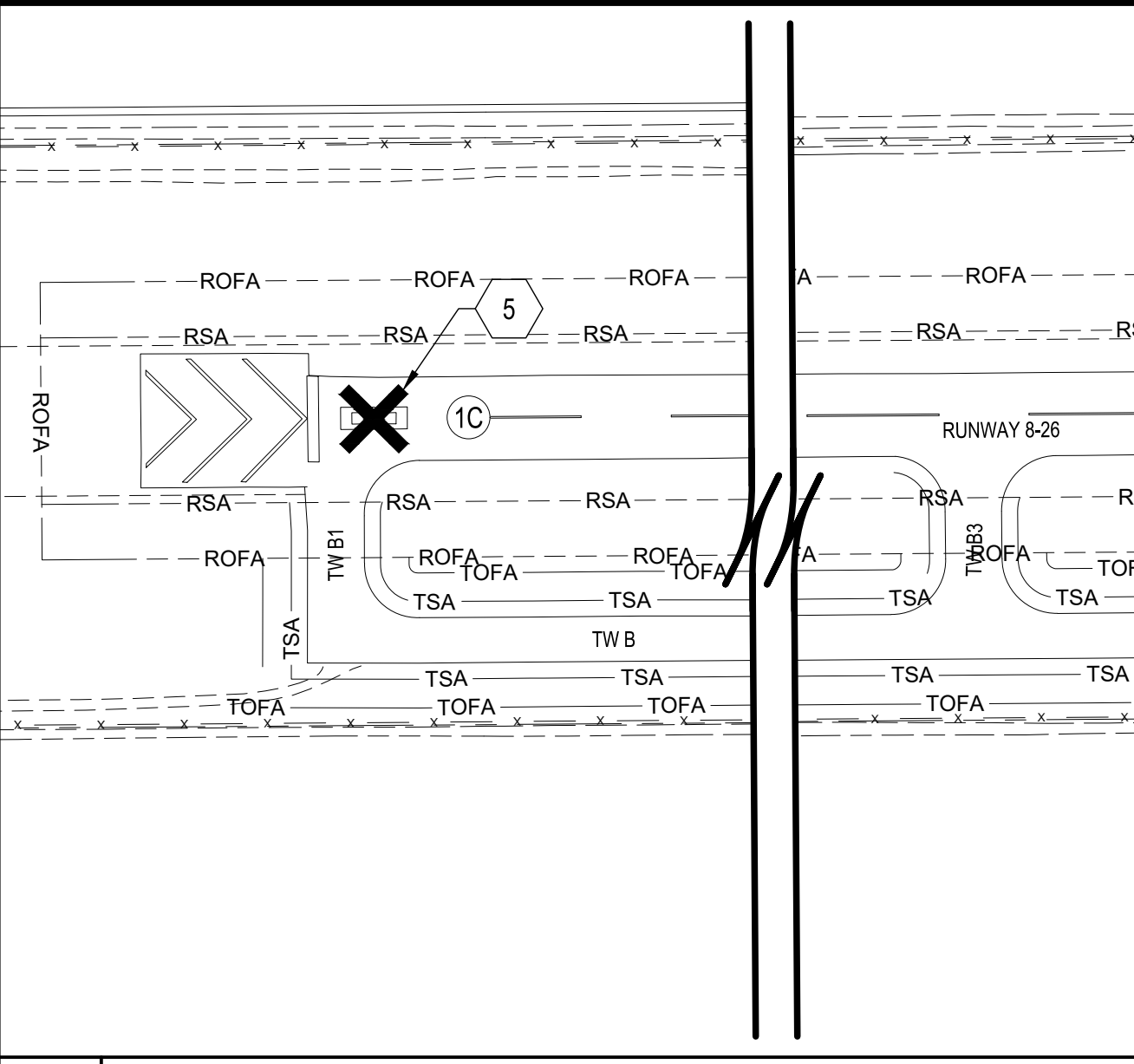
- RUNWAY 14-32 AND RUNWAY 8-26 WILL BE CLOSED DURING NIGHTTIME WORK HOURS. APRONS AND SOME TAXIWAYS WILL BE OPEN TO AIRCRAFT MOVEMENT. CONSTRUCTION VEHICLES SHALL YIELD TO TAXING AIRCRAFT AT ALL TIMES IN ALL AREAS OF THE AIRPORT.
- CONSTRUCTION VEHICLES ARE NOT PERMITTED INSIDE THE RUNWAY SAFETY AREAS THAT ARE ACTIVE AT ANY TIME DURING THIS PROJECT.
- CONTRACTOR SHALL ACCESS THE AIRPORT VIA THE AIRPORT ACCESS GATE. THE CONTRACTOR WILL KEEP LA BRUCHERIE ROAD, AND ALL ADJACENT STREETS CLEAR OF TRACK OUT AND CONSTRUCTION DEBRIS AT ALL TIMES.
- CONSTRUCTION VEHICLES MUST BE MARKED WITH AMBER BEACONS (DAY/NIGHT) OR ORANGE AND WHITE FLAGS DURING DAYLIGHT HOURS.
- CONTRACTOR SHALL ELIMINATE FOREIGN OBJECT DEBRIS PER THE CONSTRUCTION SAFETY PLAN.
- ALL PERSONAL VEHICLES SHALL BE PARKED IN THE STAGING AREA.
- THE STAGING AREA IS NOT FENCED OR SECURED. FENCING & SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR SHALL COMPLY WITH THE CONSTRUCTION SAFETY PLAN IN SECTION 70 OF THE CONTRACT DOCUMENTS.
- ALL SPOIL MATERIAL WHICH IS SUITABLE FOR EMBANKMENT SHALL BE DISPOSED AS DIRECTED BY THE AIRPORT AUTHORITIES. SPOIL MATERIAL WHICH IS NOT SUITABLE FOR EMBANKMENT SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
- AIRPORT UNICOM FREQUENCY = 122.7
- MAXIMUM EQUIPMENT HEIGHT = 12 FEET
- ALLOWED CONSTRUCTION TIME = 60 CALENDAR DAYS

1. CONTRACTOR'S ACCESS TO STAGING AREA
2. CONTRACTOR'S ACCESS TO WORK AREAS
3. WORK AREA "1A"
4. WORK AREA "1B"
5. TEMPORARY LIGHTED CLOSED RUNWAY MARKING, INCIDENTAL TO ITEM C-106. SEE DETAIL A3/GC501
6. TEMPORARY INTERLOCKING BARRICADES, ITEM C-106 (TYP.). SEE DETAIL A1/GC501
7. CONTRACTOR PROVIDED GATE GUARD, INCIDENTAL TO ITEM C-106
8. CONTRACTOR'S STAGING AREA
9. WORK AREA "1C"

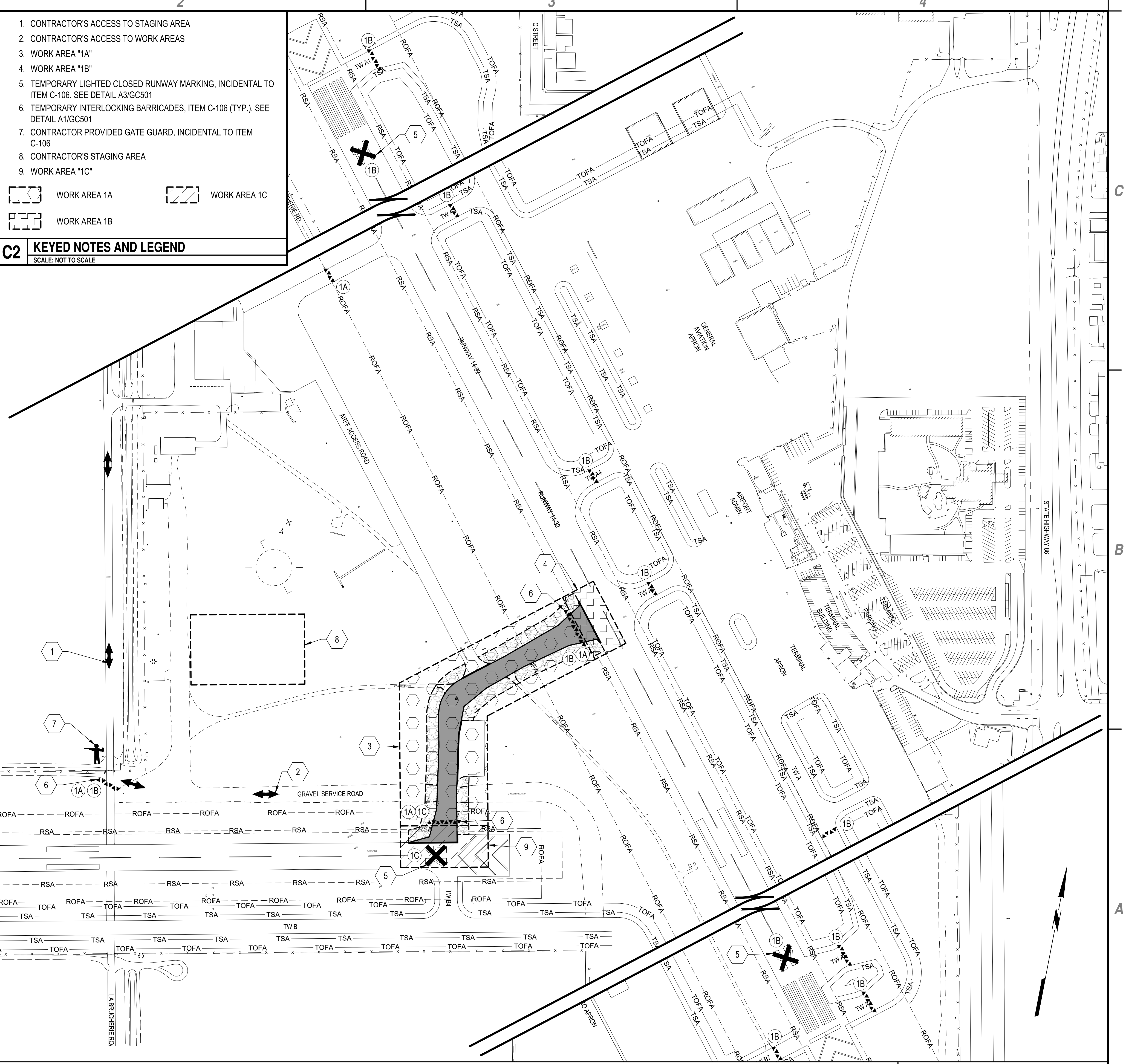
C2 KEYED NOTES AND LEGEND
SCALE: NOT TO SCALE



B1 GENERAL NOTES
SCALE: NOT TO SCALE

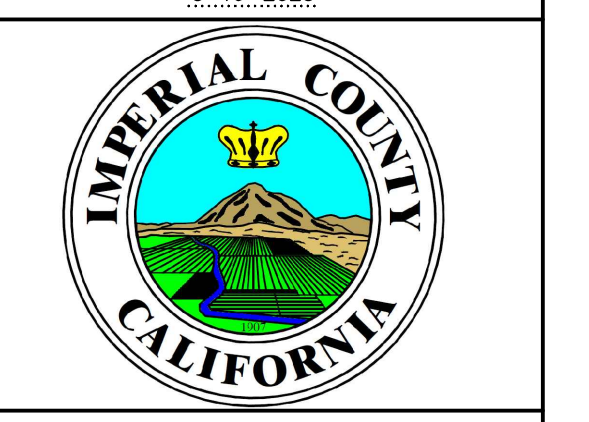
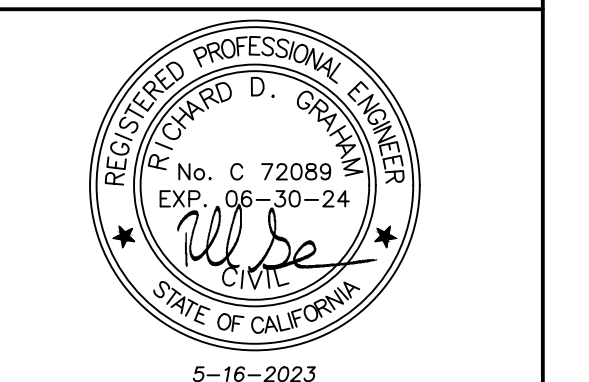


A1 CONSTRUCTION SAFETY PHASING PLAN
SCALE: 1" = 150'



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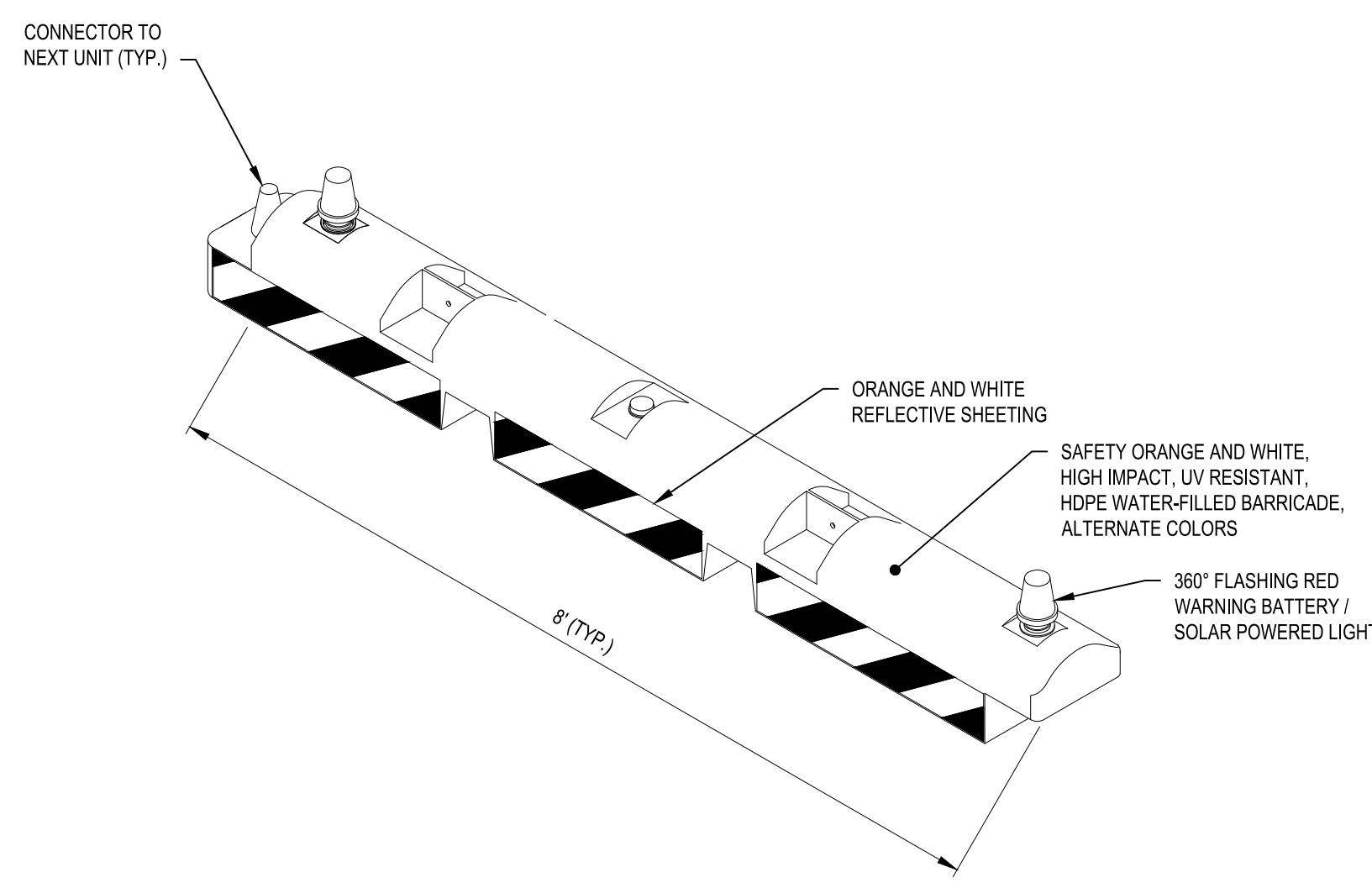
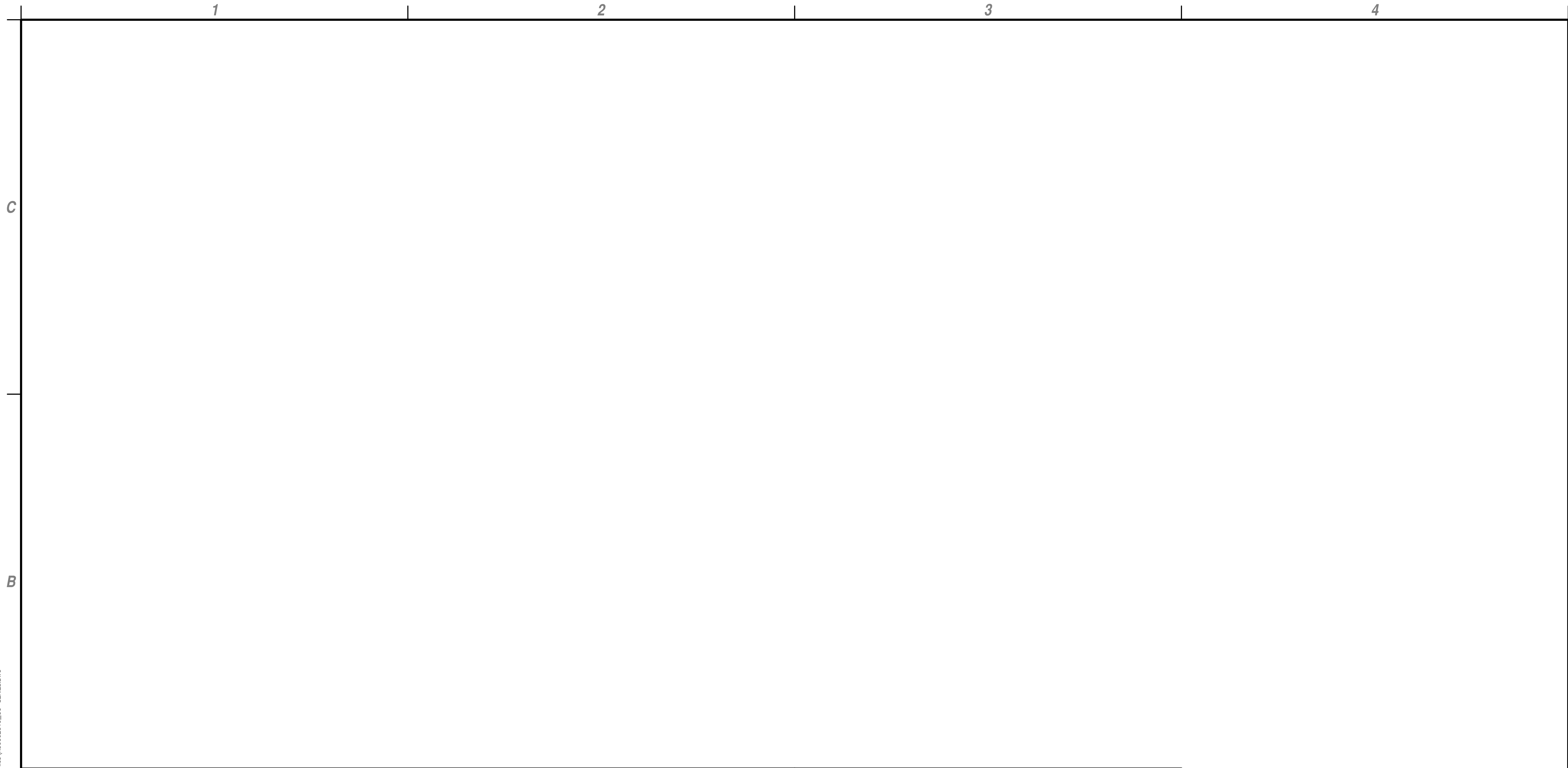
IMPERIAL COUNTY AIRPORT
 PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5

MARK	DATE	DESCRIPTION
REVISIONS		
		PROJECT NO: K30002019
		DATE: MAY 2023
		DRAWN BY: G.C. HAYDEN
		DESIGNED BY: M.E. BARR
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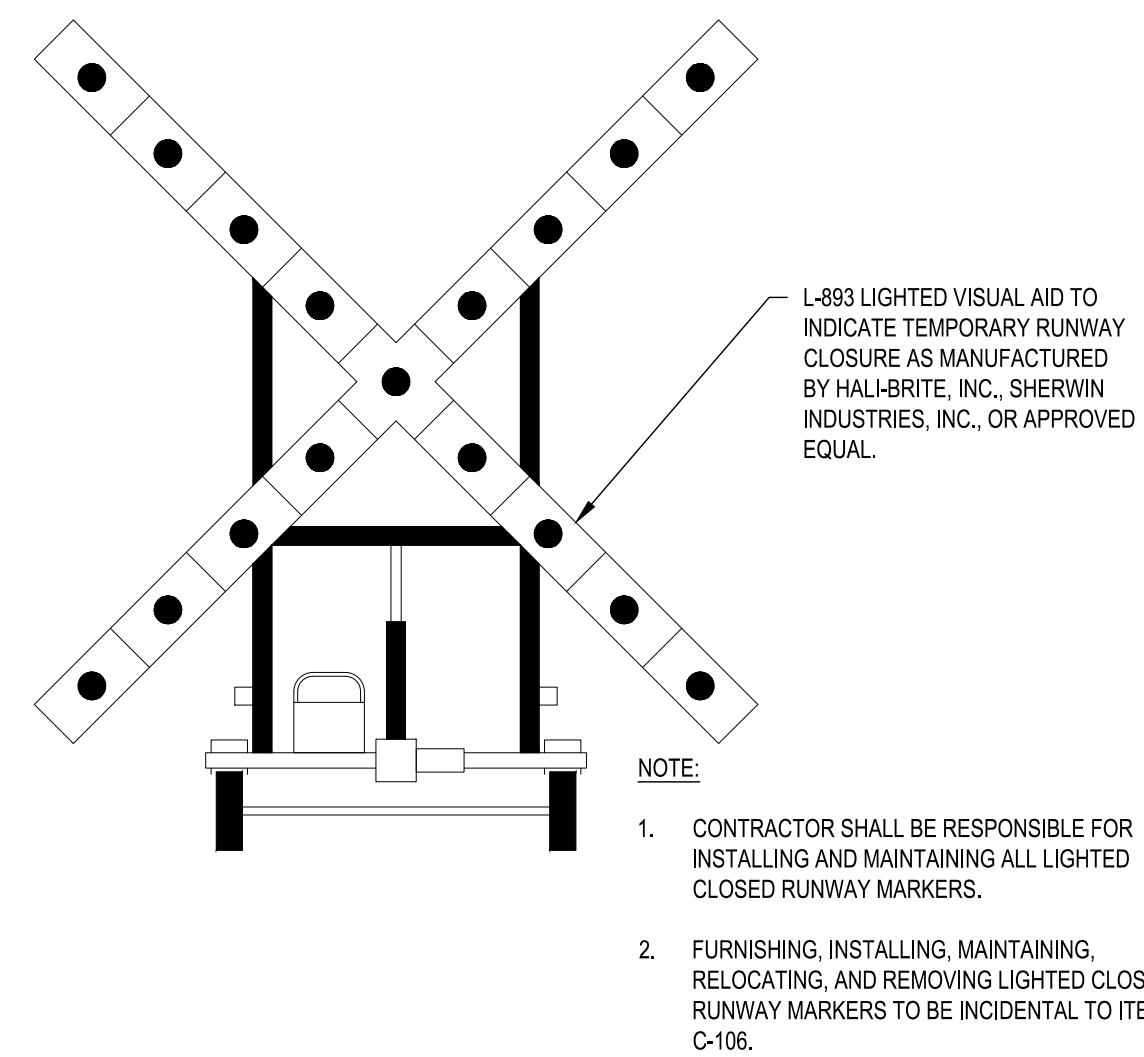
CONSTRUCTION SAFETY PHASING PLAN

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- BARRICADE NOTES:**
- BARRICADES SHALL BE 6" X 8' AIRPORT LOW PROFILE BARRICADE AS MANUFACTURED BY NEUBERT AERO CORP., OR 10" X 8' RRM LOW PROFILE AIRPORT BARRIER AS MANUFACTURED BY SHERWIN INDUSTRIES, INC., OR APPROVED EQUAL.
 - BARRICADES SHALL EXTEND TO EDGES OF PAVEMENT UNLESS OTHERWISE SHOWN. EXACT LOCATIONS SHALL BE DETERMINED DURING CONSTRUCTION.
 - BARRICADES SHALL BE FILLED WITH WATER AND INTERLOCKED WITH EACH OTHER. BARRICADES WHICH HAVE TO BE MOVED DAILY MAY BE ANCHORED TO THE PAVEMENT BY AN ALTERNATE METHOD.
 - WARNING LIGHTS SHALL BE ATTACHED TO EACH BARRICADE.
 - BARRICADES SHALL BE SET BACK 2 FEET FROM THE LIMITS OF CONSTRUCTION.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING PROPER POSITIONING OF ALL BARRICADES.

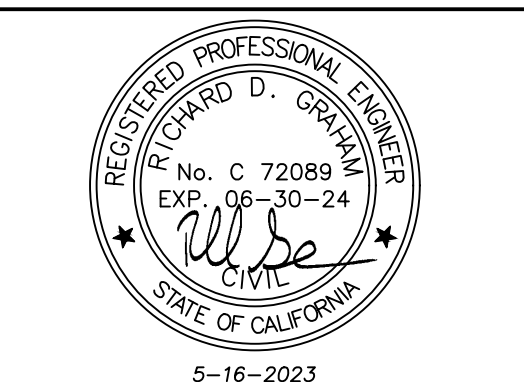


A1 INTERLOCKING BARRICADE DETAIL
SCALE: NOT TO SCALE

A3 LIGHTED CLOSED RUNWAY MARKING DETAIL
SCALE: NOT TO SCALE



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IMPERIAL COUNTY AIRPORT
PAVEMENT REHABILITATION
(APMS 1 & 2) TAXIWAY B4/C5

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CHECKED BY: S.L. UNDERWOOD		

**CONSTRUCTION
SAFETY PHASING
DETAILS**

GC501
7 of 21

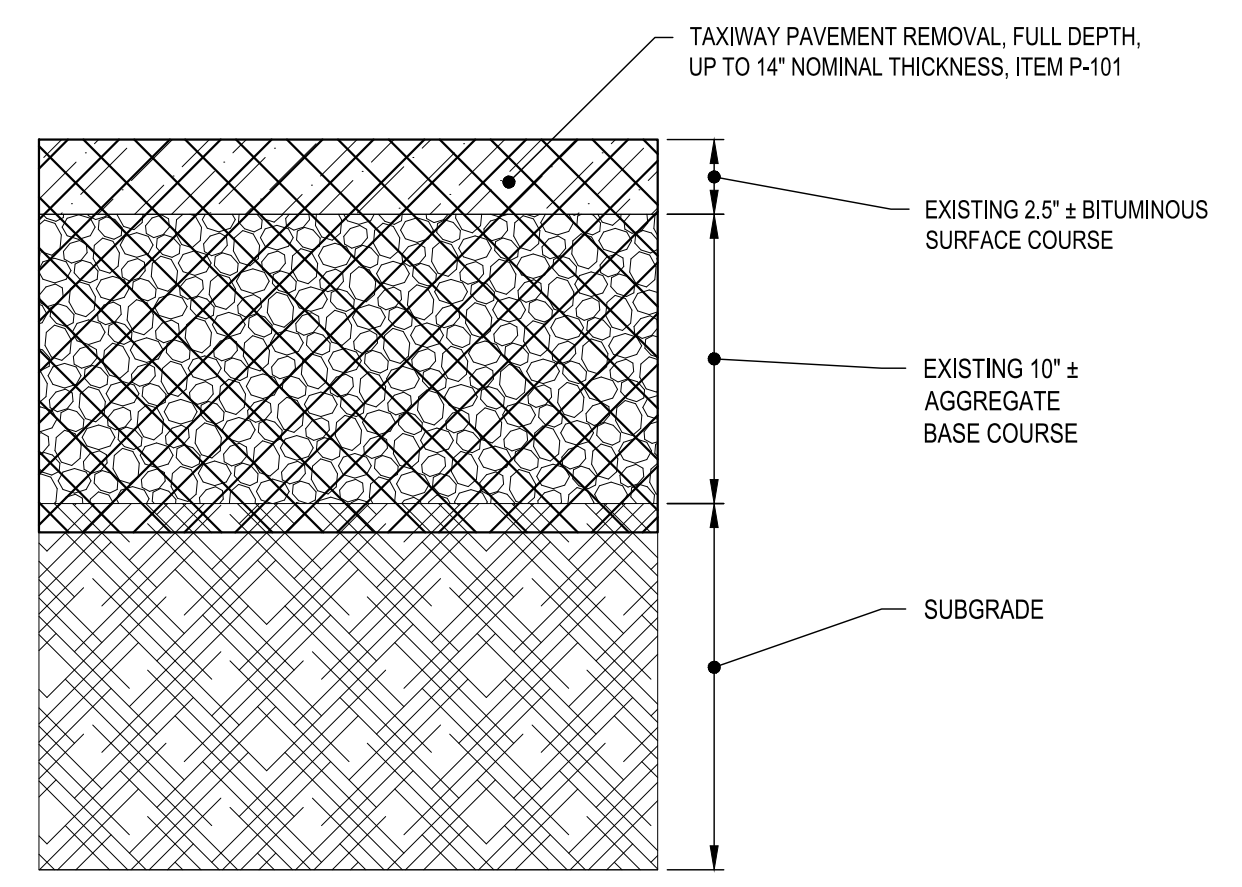
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B1 DEMOLITION PLAN AND DETAILS
SCALE: 1" = 40'

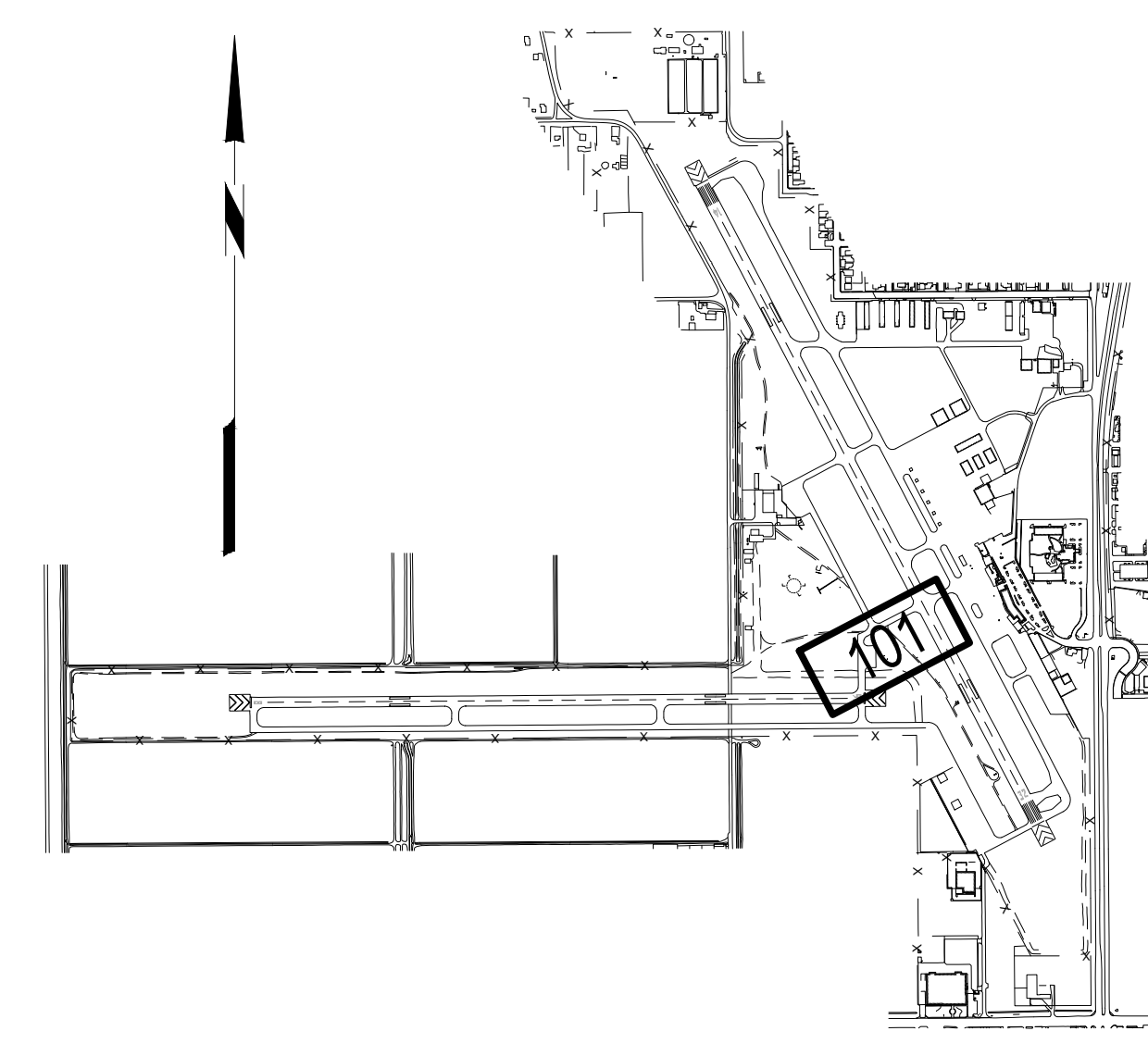
- 100. SAWCUT REMOVE AND REPLACE 2" OF EXISTING ASPHALTIC CONCRETE PAVEMENT, ITEM P-101.
- 102. TAXIWAY ASPHALT CONCRETE PAVEMENT REMOVAL, FULL DEPTH, UP TO 11.5" NOMINAL THICKNESS, ITEM P-101, SEE TYPICAL SECTIONS A3/CD101.
- 103. LIMITS OF FULL DEPTH PAVEMENT EXCAVATION, SEE TYPICAL SECTION B2/CG301 AND CG SERIES FOR GRADING LIMITS, ITEM P-152
- 105. FULL DEPTH SAWCUT IN EXISTING PAVEMENT, INCIDENTAL TO ITEM P-101.
- 106. REMOVE EXISTING AIRFIELD GUIDANCE SIGN AND RECONNECT SIGN CIRCUIT AS DIRECTED BY THE RPR, ITEM L-125.
- 107. EXISTING DOMESTIC WATER MAIN TO REMAIN, PROTECT IN PLACE
- 110. EXISTING ELECTRICAL STRUCTURE TO REMAIN, PROTECT IN PLACE
- 112. REMOVE EXISTING AIRFIELD GUIDANCE SIGN AND FOUNDATION, ITEM L-125.
- 113. REMOVE EXISTING ELEVATED EDGE LIGHT AND FOUNDATION, ITEM L-125.
- 118. REMOVE EXISTING ELECTRICAL CONDUIT, ITEM L-110
- 119. EXISTING ELECTRICAL PULLBOX TO BE MODIFIED, ITEM L-115. SEE EL-SERIES PLANS FOR DETAILS
- 120. LIMIT OF CONDUIT REMOVAL, ITEM L-125
- 121. EXISTING ELECTRICAL DUCT BANK TO REMAIN, PROTECT IN PLACE
- 122. EXISTING COMMUNICATION LINE TO REMAIN, PROTECT IN PLACE
- 123. EXISTING DRAINAGE STRUCTURE TO REMAIN, PROTECT IN PLACE
- 124. EXISTING STORM LINE TO REMAIN, PROTECT IN PLACE
- 125. OBLITERATE EXISTING TAXIWAY CENTERLINE MARKING, ITEM P-620

A1 KEYED NOTES
SCALE: NOT TO SCALE

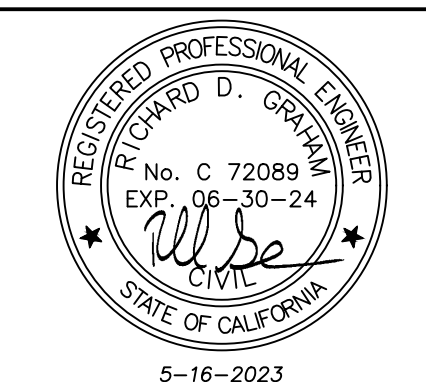
A3 TYPICAL TAXIWAY PAVEMENT DEMOLITION SECTION
SCALE: NOT TO SCALE



A4 KEY MAP
SCALE: NOT TO SCALE



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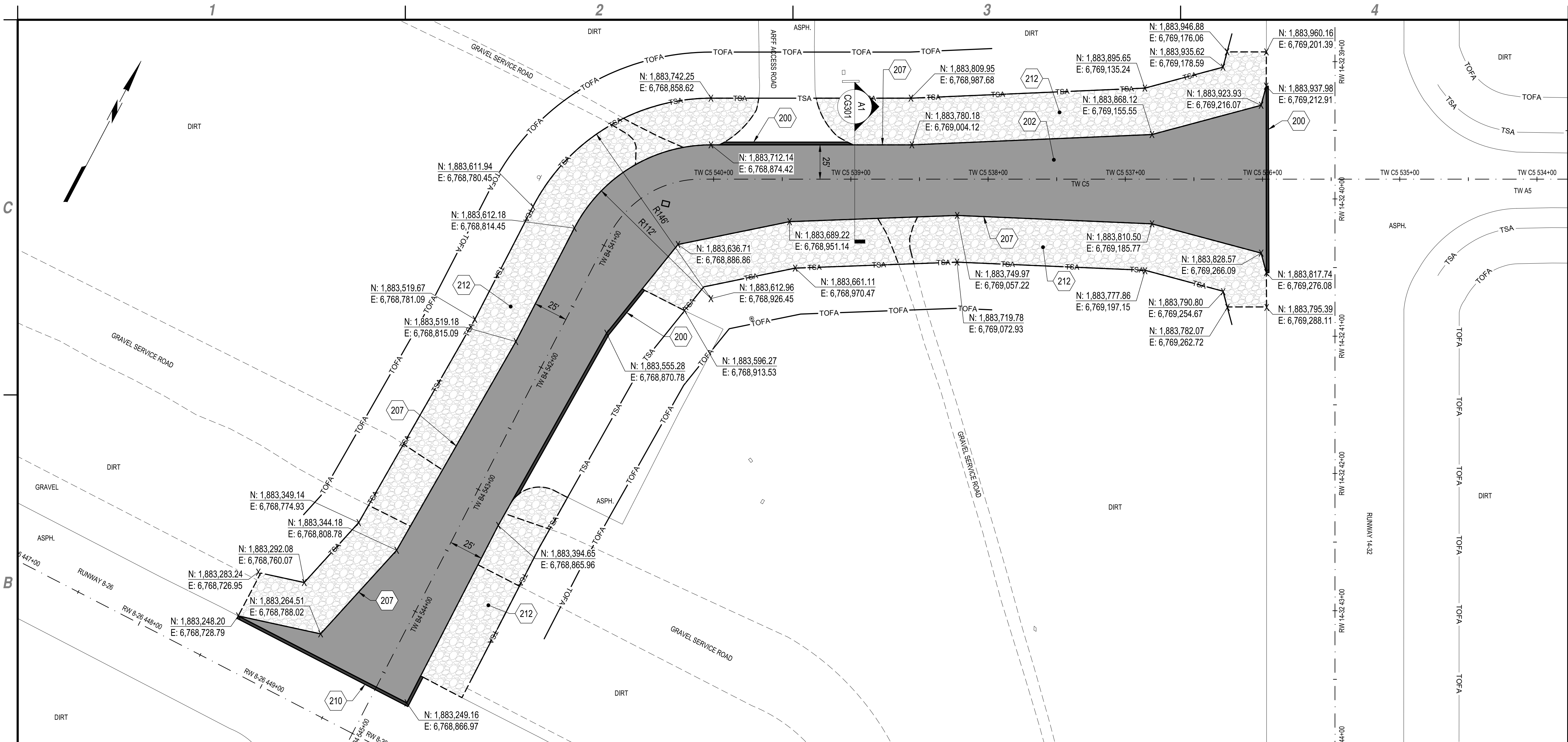
PAVEMENT REHABILITATION
(APMS 1 & 2) TAXIWAY B4/C5

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DEMOLITION PLAN AND DETAILS

CD101
8 of 21

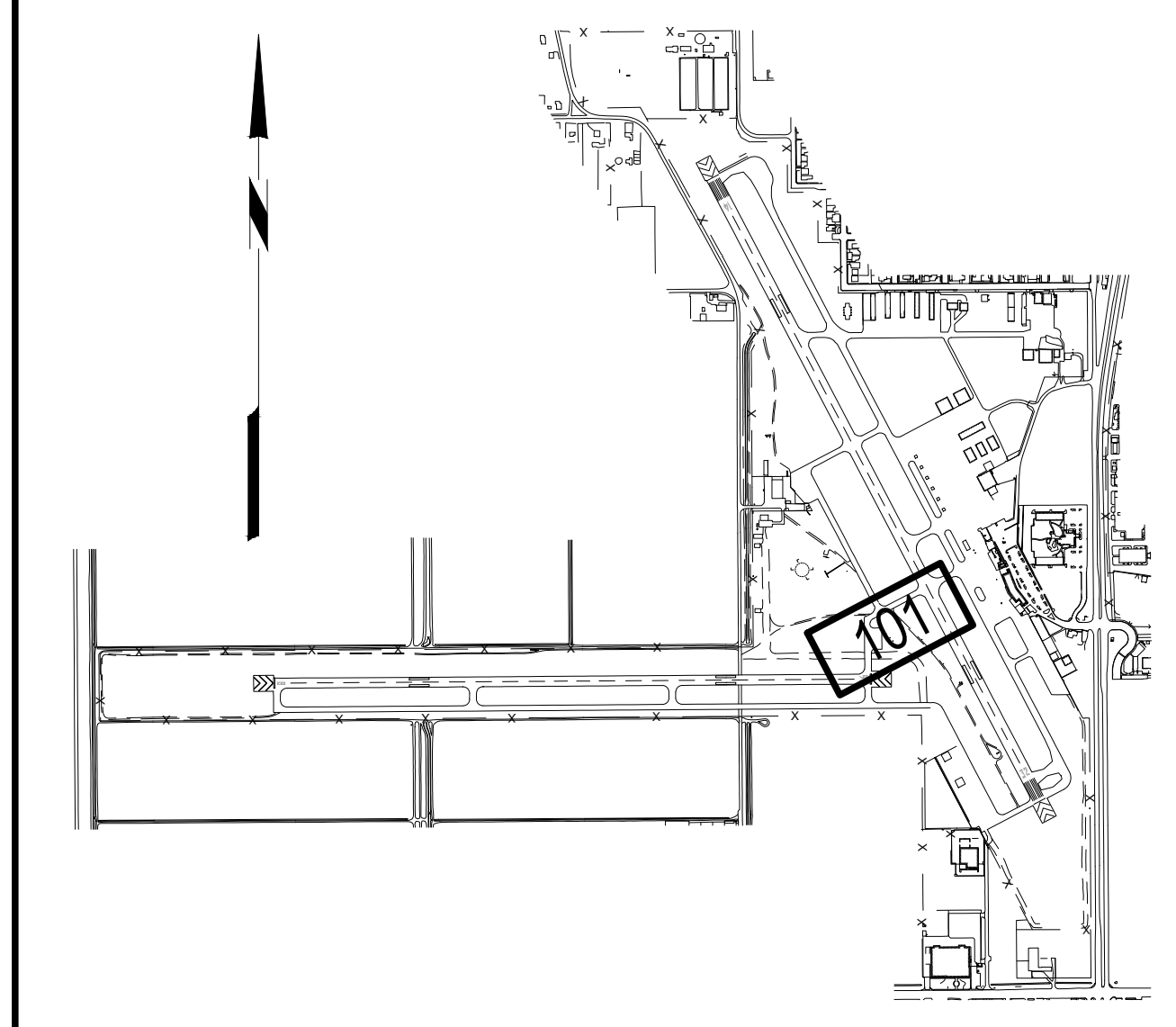
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B1 GEOMETRY PLAN
 SCALE: 1" = 40'

- 200. MATCH EXISTING EDGE OF PAVEMENT, SEE DETAIL C2/CG301
- 202. BITUMINOUS CONCRETE TAXIWAY PAVEMENT, ITEM P-401, SEE TYPICAL SECTION A1/CG301
- 207. EDGE OF PAVEMENT, SEE DETAIL B1/CG301.
- 210. KEY PROPOSED PAVEMENT INTO EXISTING PAVEMENT SECTION, SEE DETAIL B3/CP501
- 212. 4" CRUSHED AGGREGATE ON PREPARED SUBGRADE, ITEM P-218, SEE DETAIL C1/CG301

A1 KEYED NOTES
 SCALE: NOT TO SCALE



A4 KEY MAP
 SCALE: NOT TO SCALE



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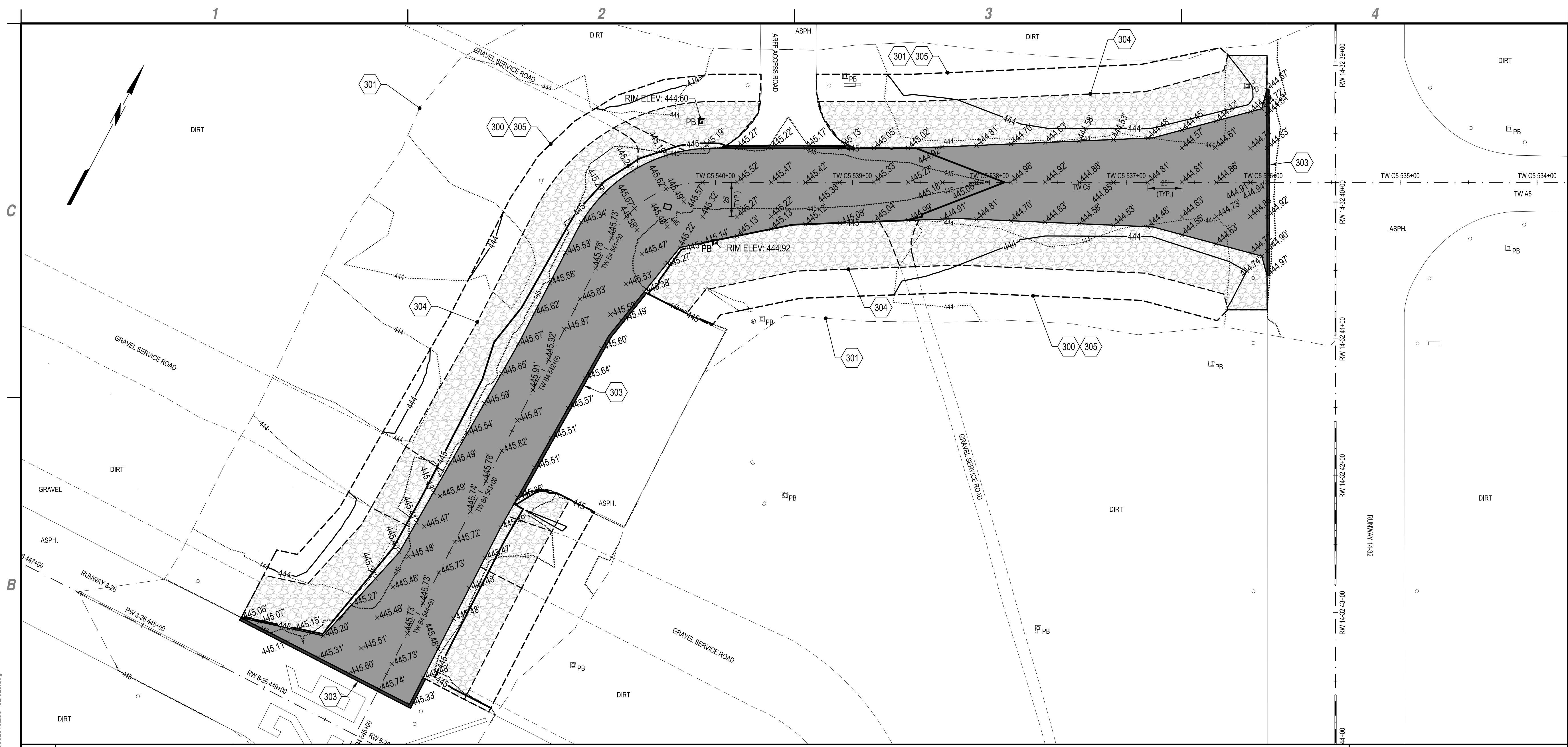


IMPERIAL COUNTY AIRPORT
**PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5**

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GEOMETRY PLAN

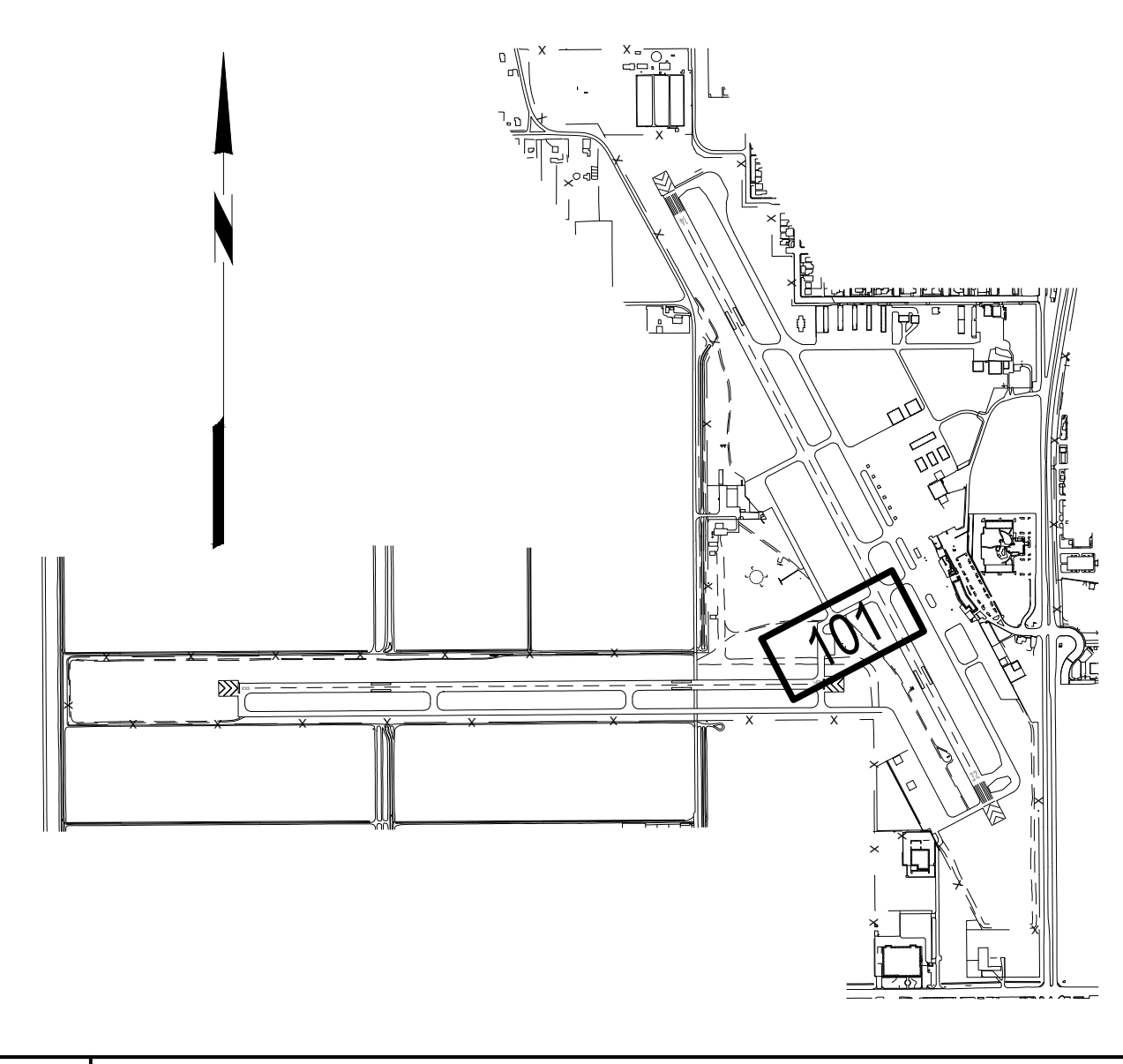
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B1 GRADING PLAN
 SCALE: 1" = 40'



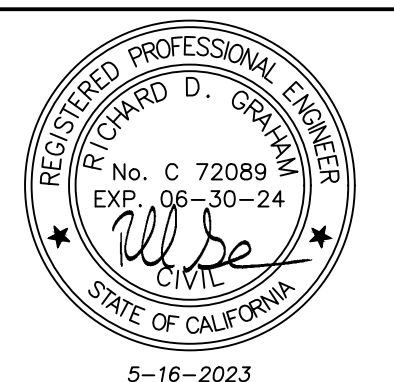
- A1 KEYED NOTES**
 SCALE: NOT TO SCALE
- 300. GRADING LIMIT LINE.
 - 301. SURVEY LIMIT LINE.
 - 303. MATCH EXISTING PAVEMENT, SEE DETAIL C2/CG301, (TYP.)
 - 304. GRADE TO -1%, OTHERWISE AS DIRECTED BY RPR, ITEM P-152
 - 305. MEET EXISTING GRADE, ITEM P-152



A4 KEY MAP
 SCALE: NOT TO SCALE



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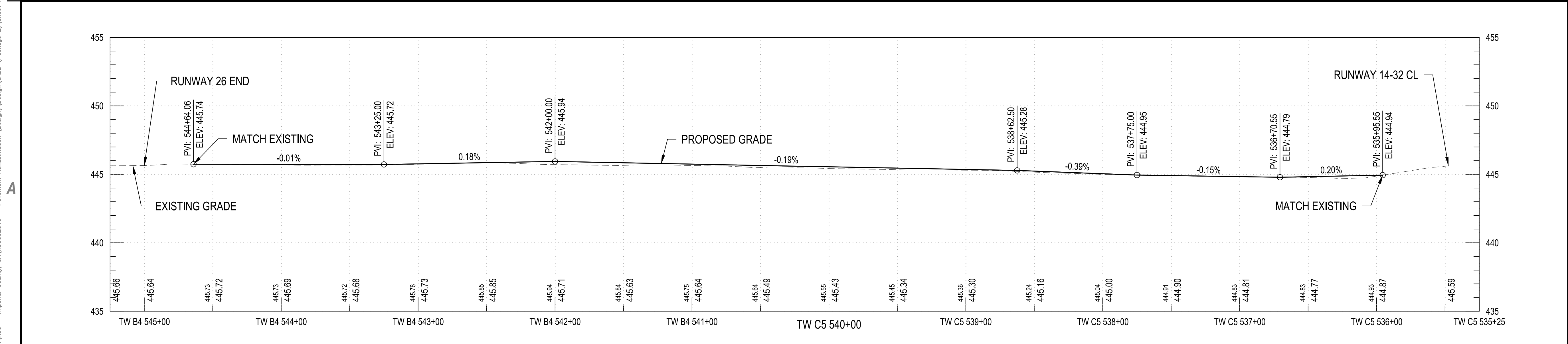
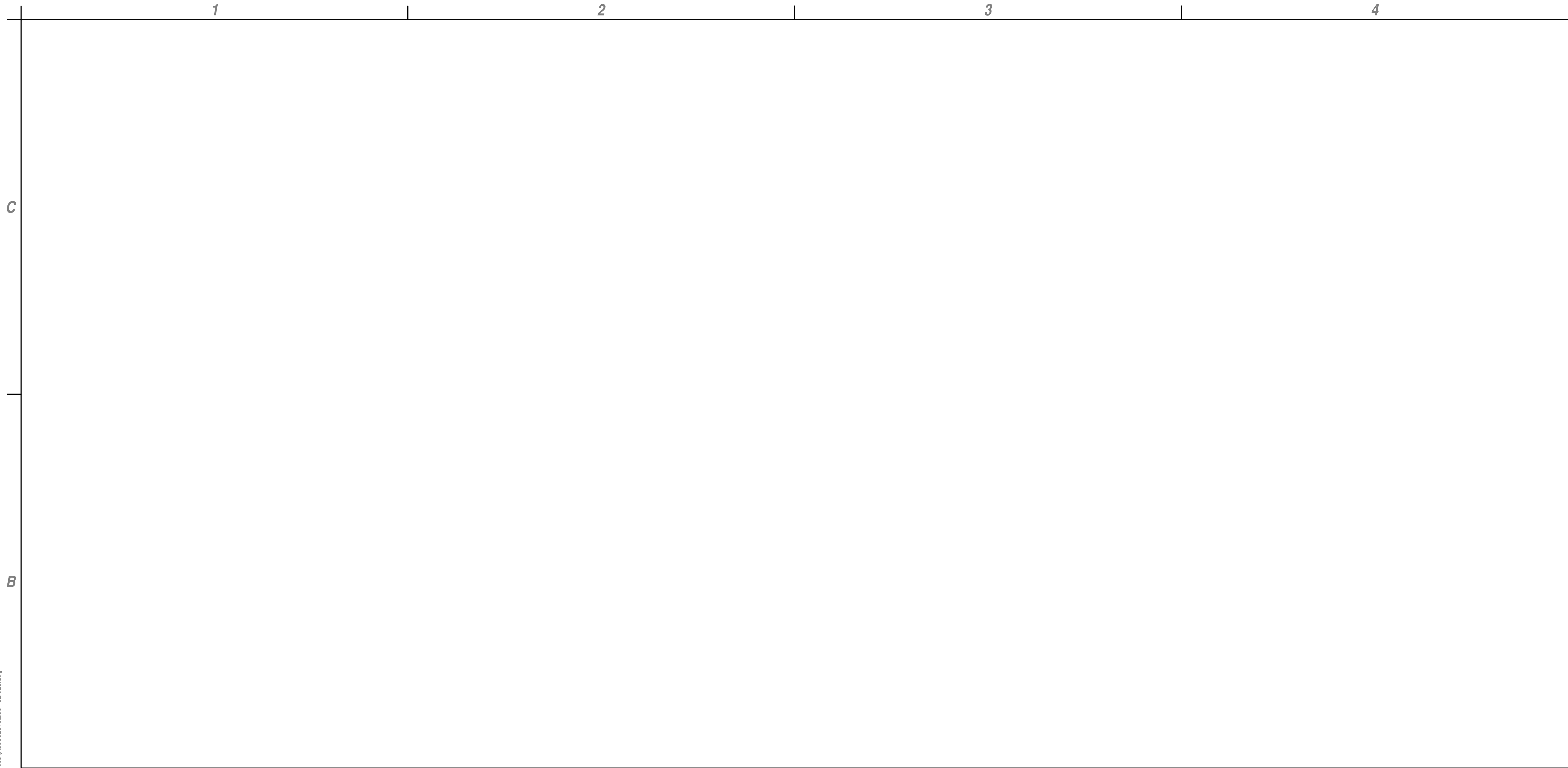
IMPERIAL COUNTY AIRPORT
**PAVEMENT REHABILITATION
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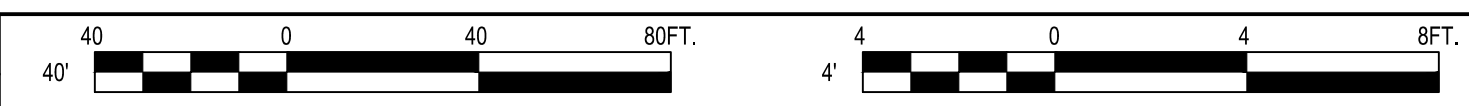
GRADING PLAN

CG101
 10 of 21

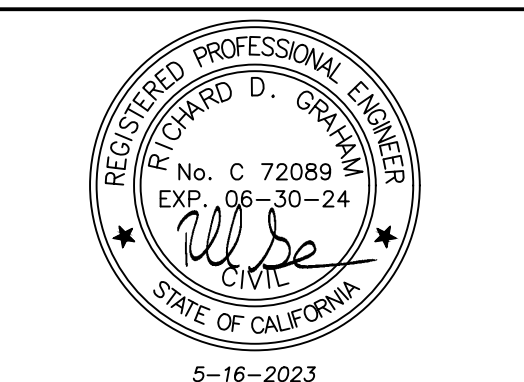
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A1 PROFILE
 SCALE: 1" = 40' HORIZ, 1" = 4' VERT.



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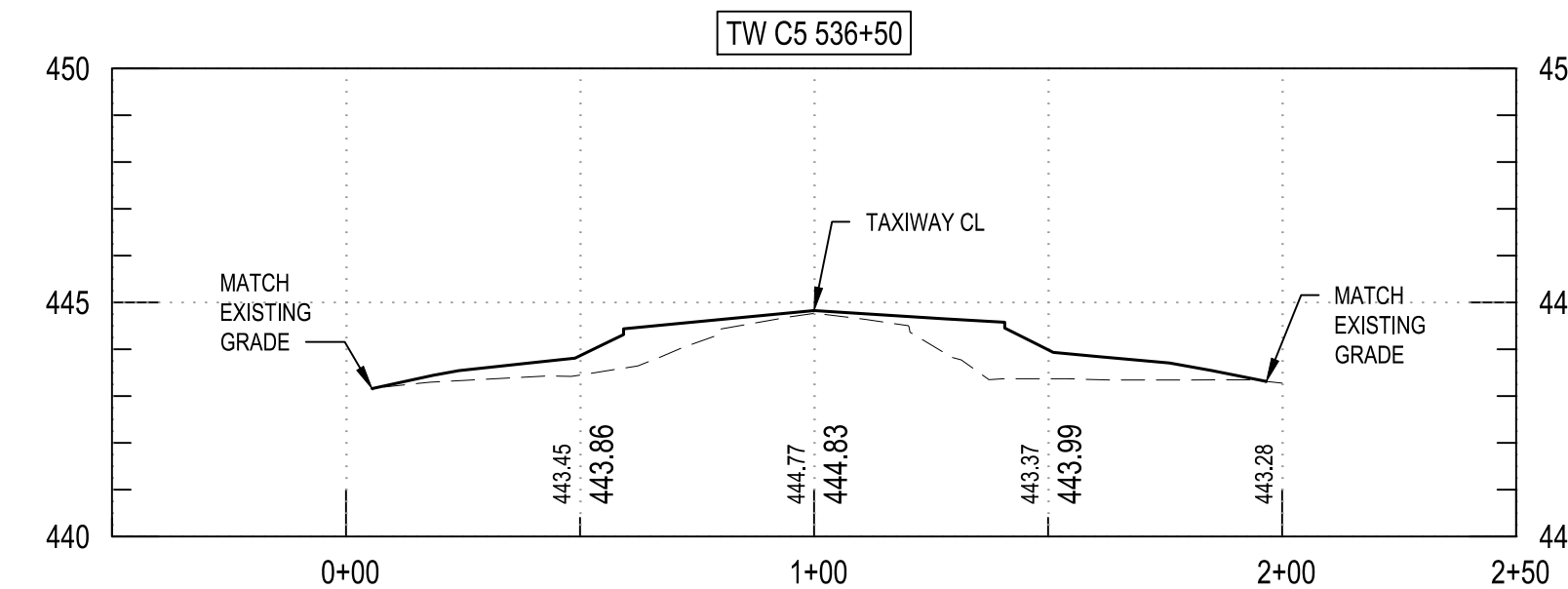
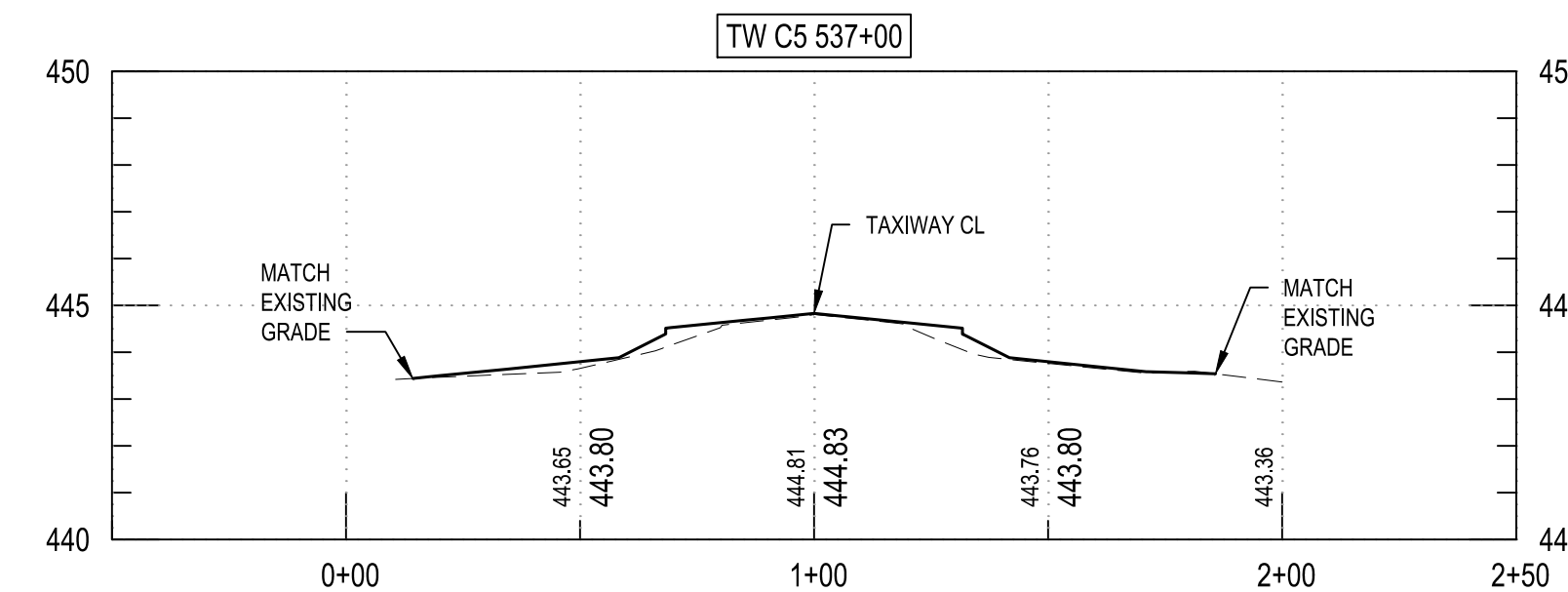
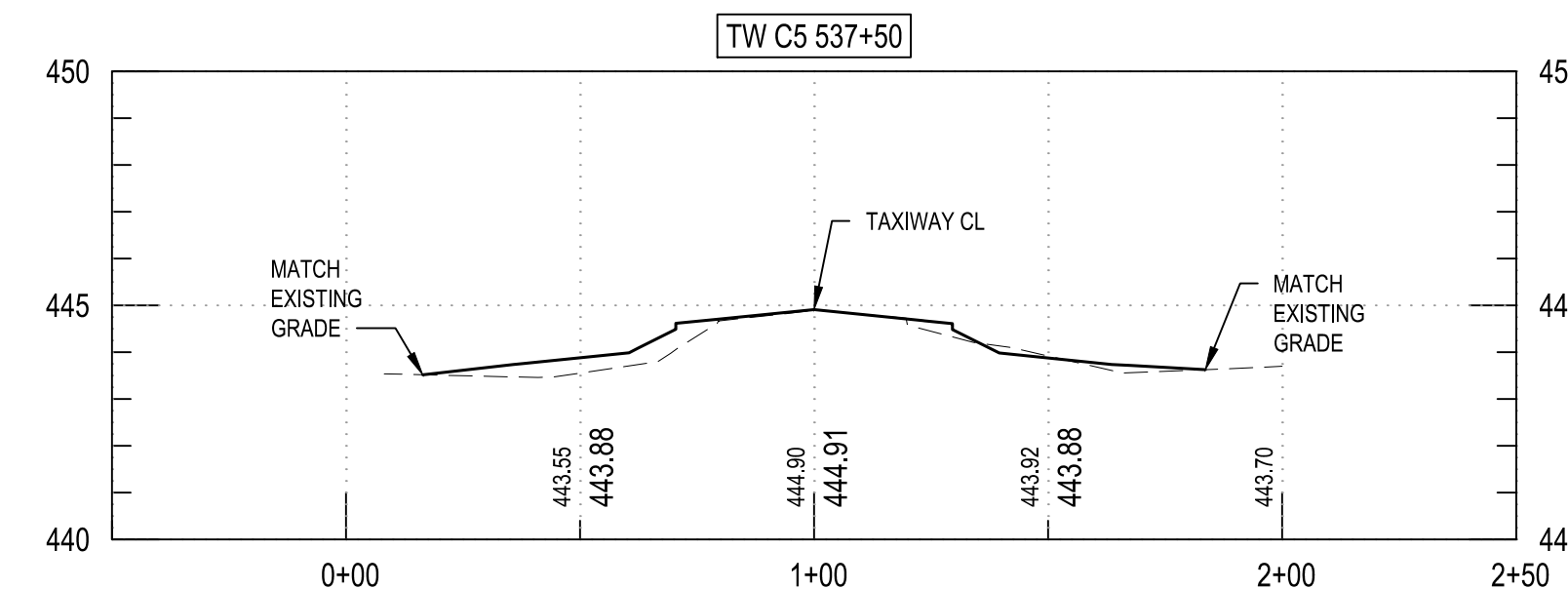
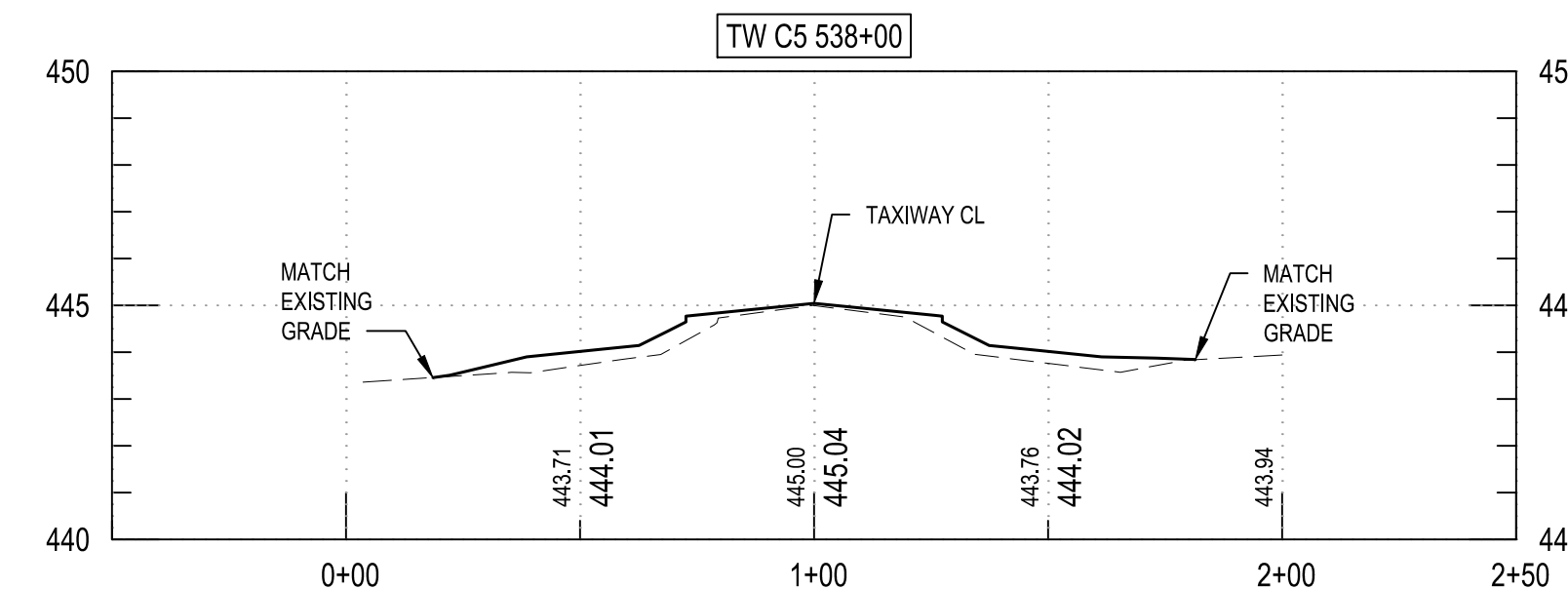
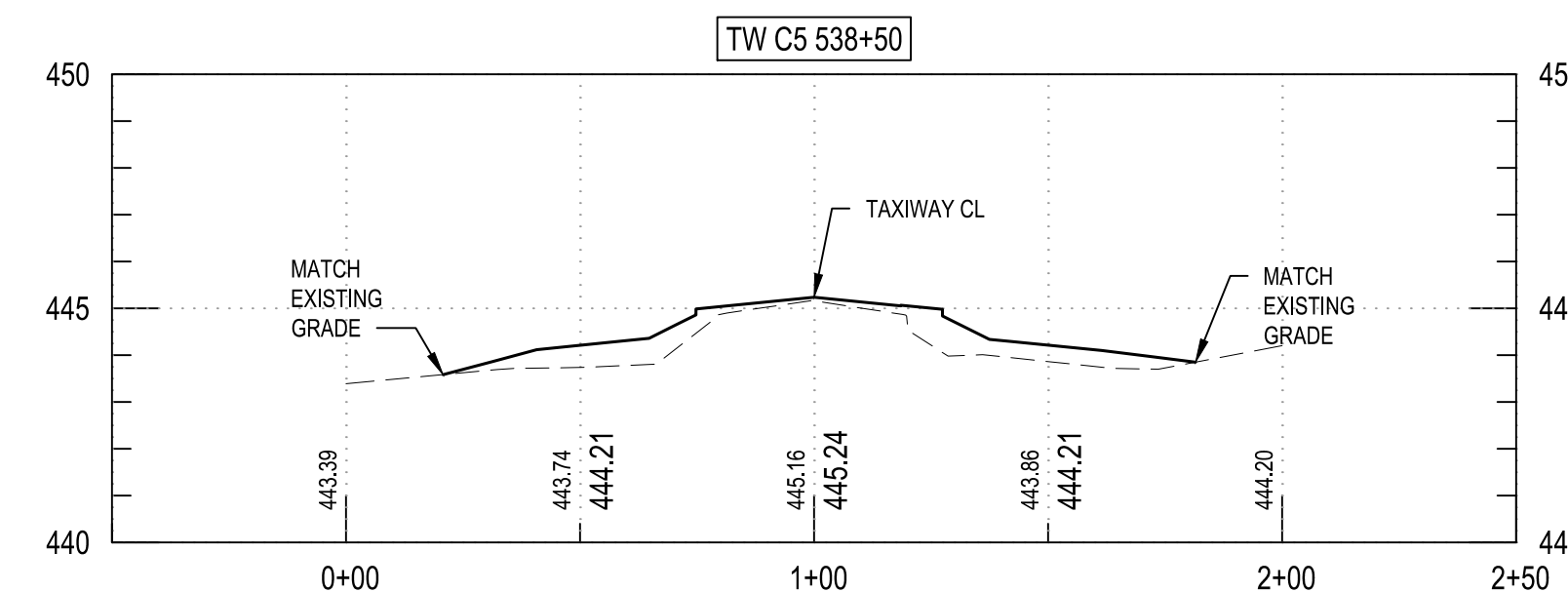
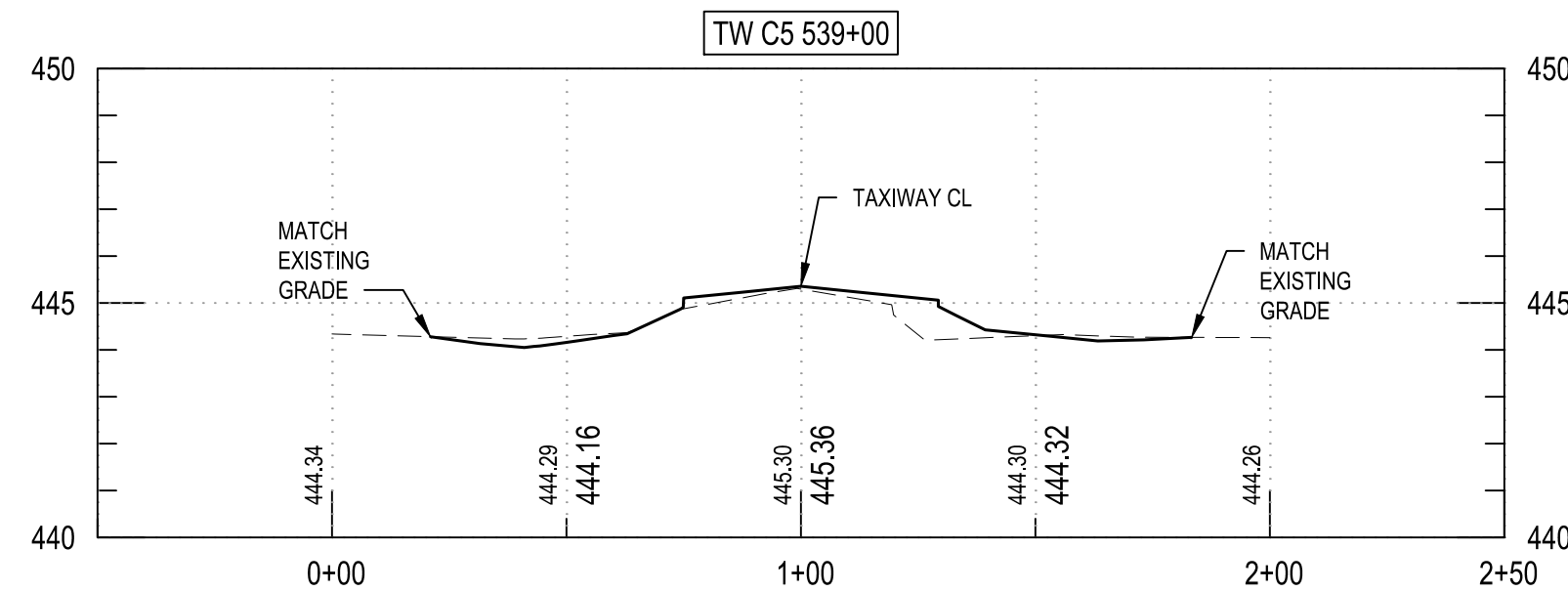
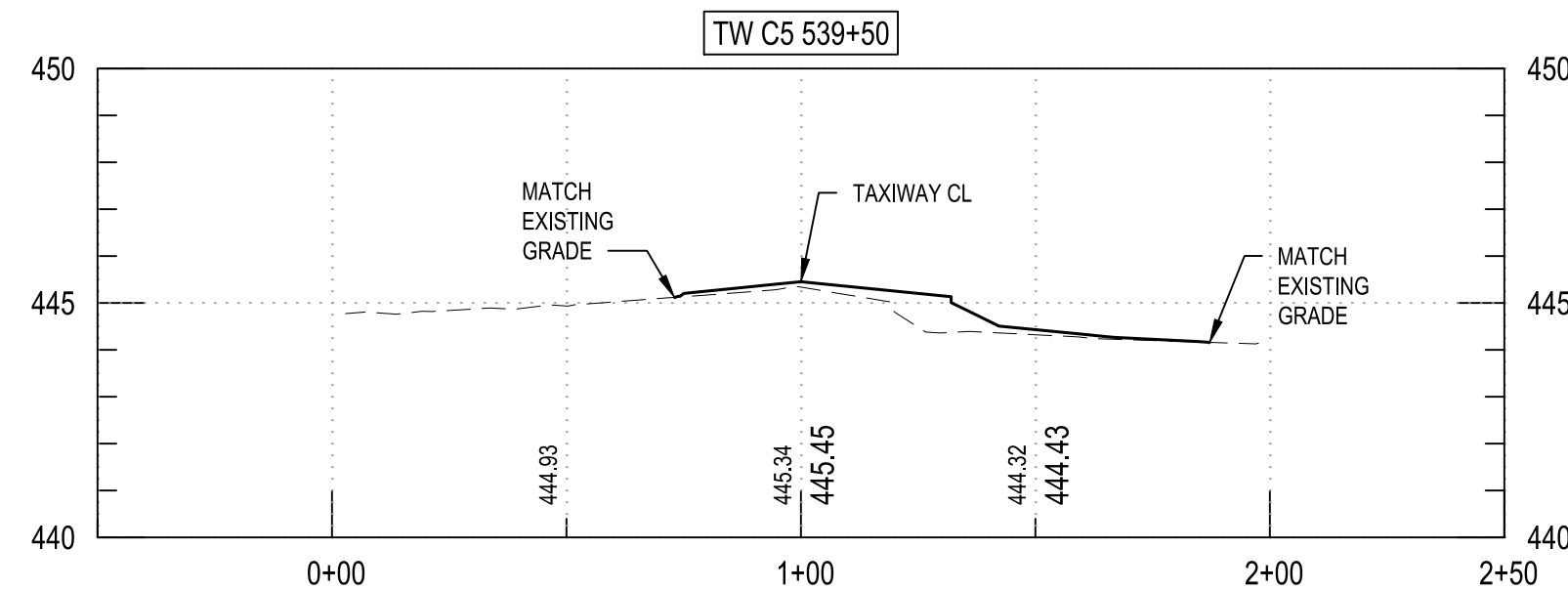
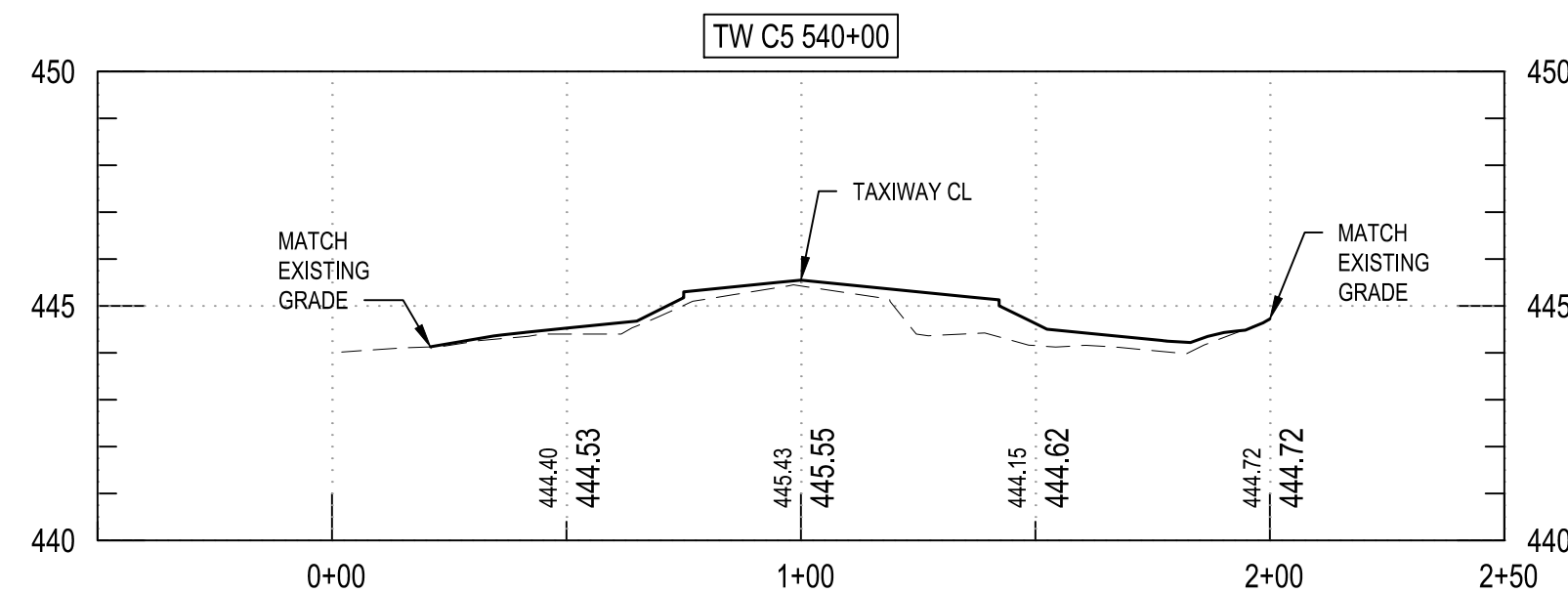
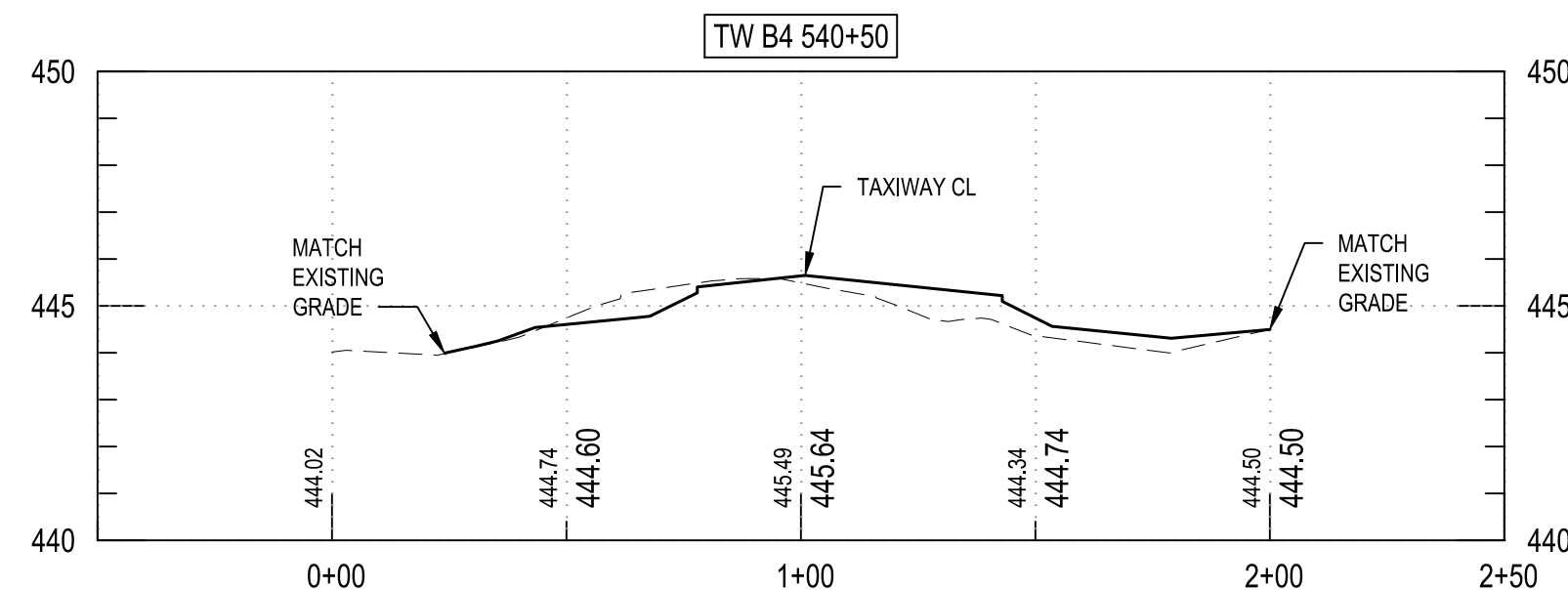
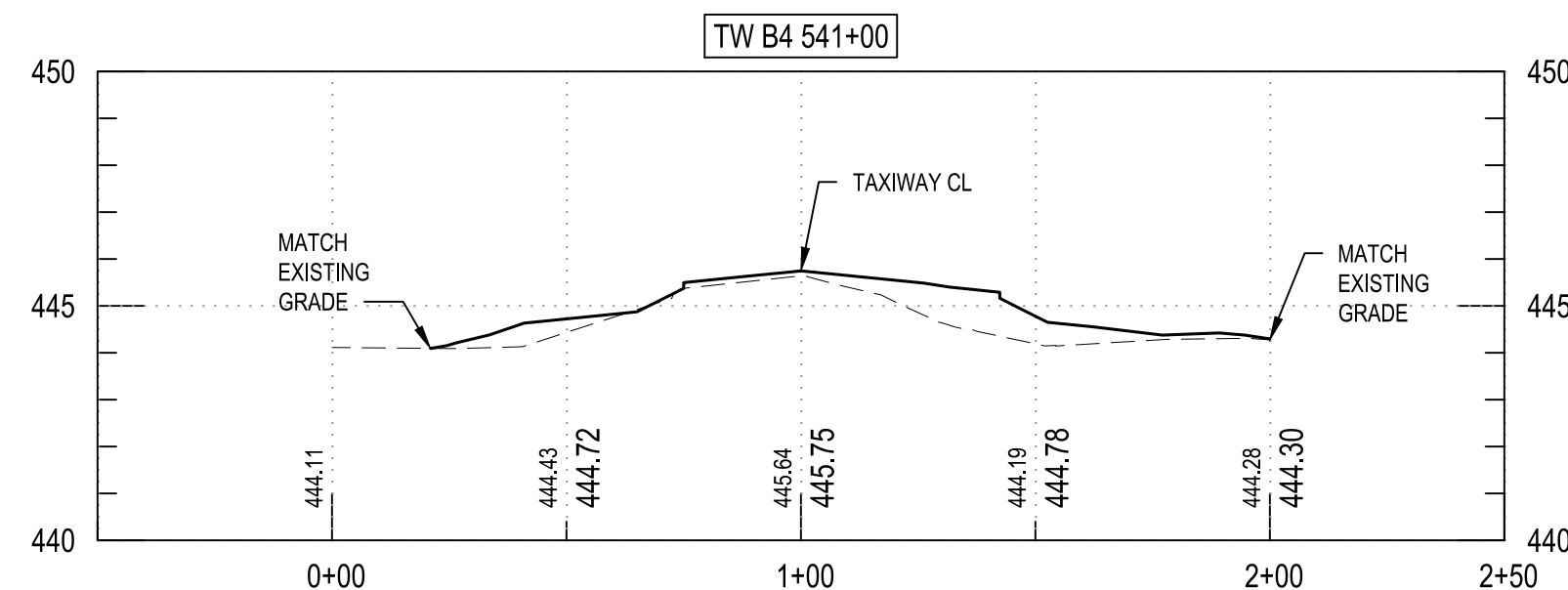
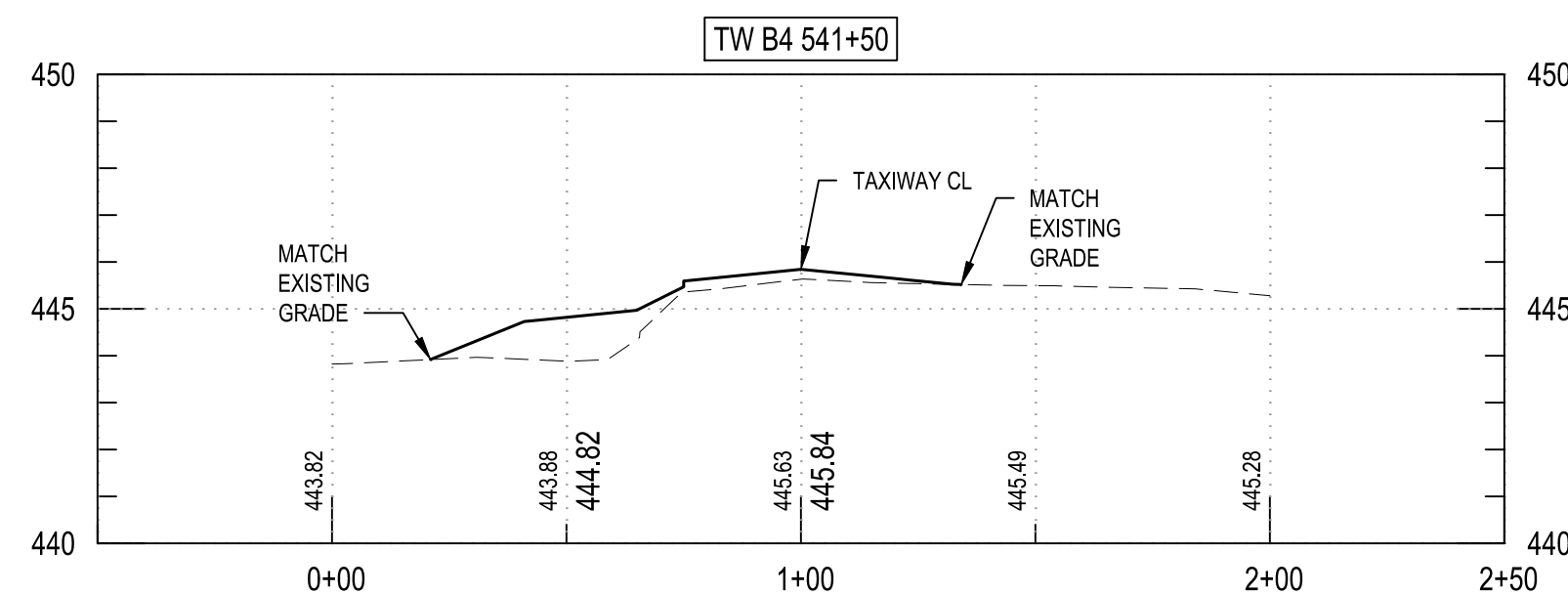
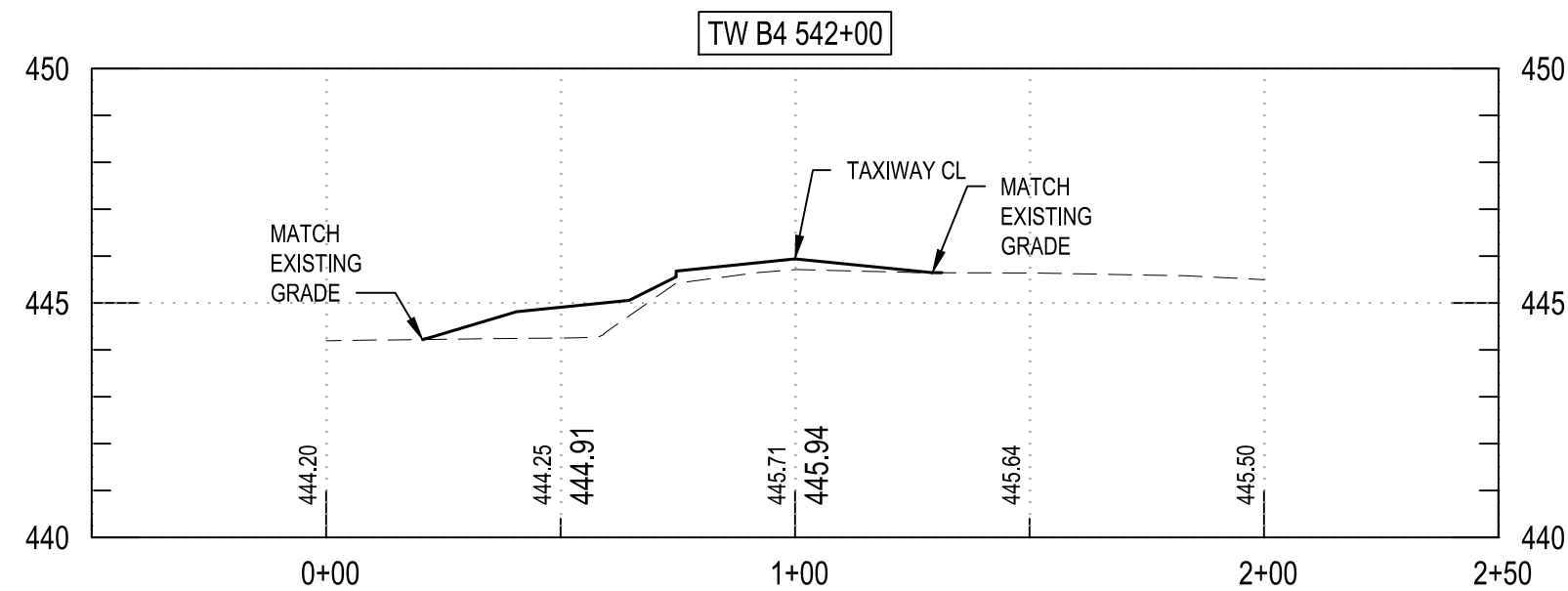
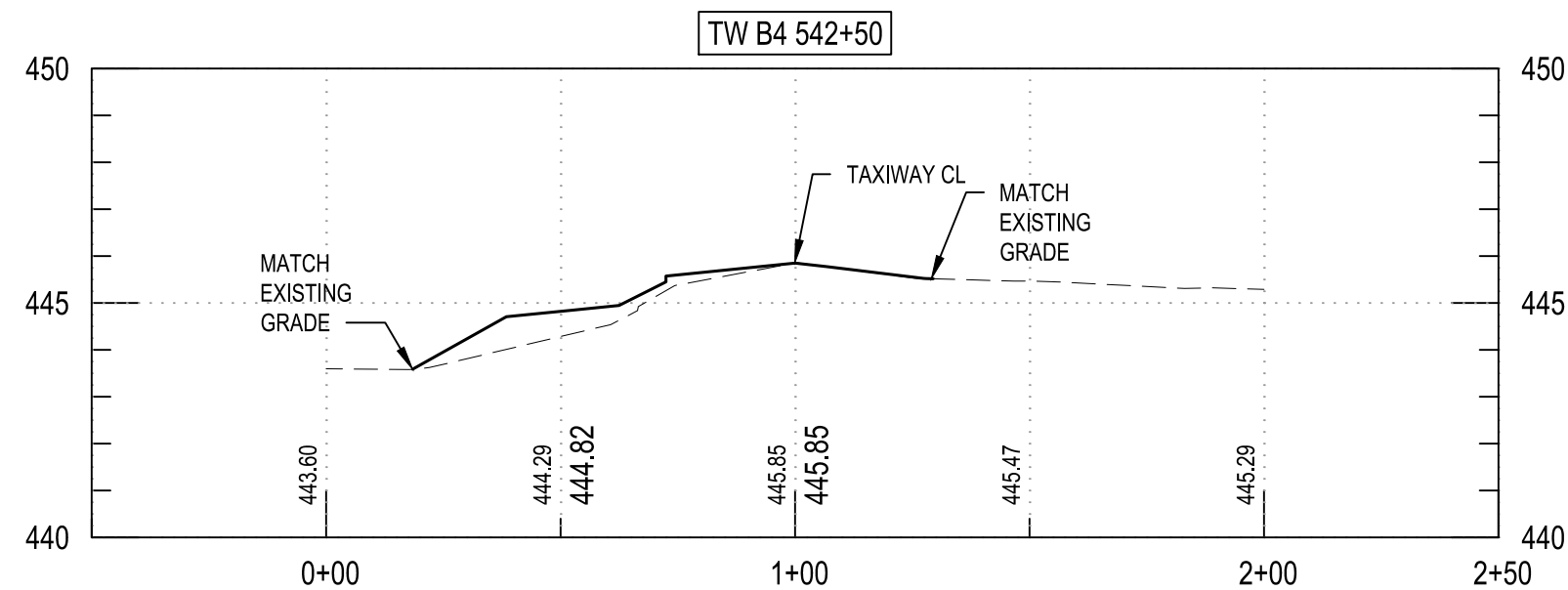
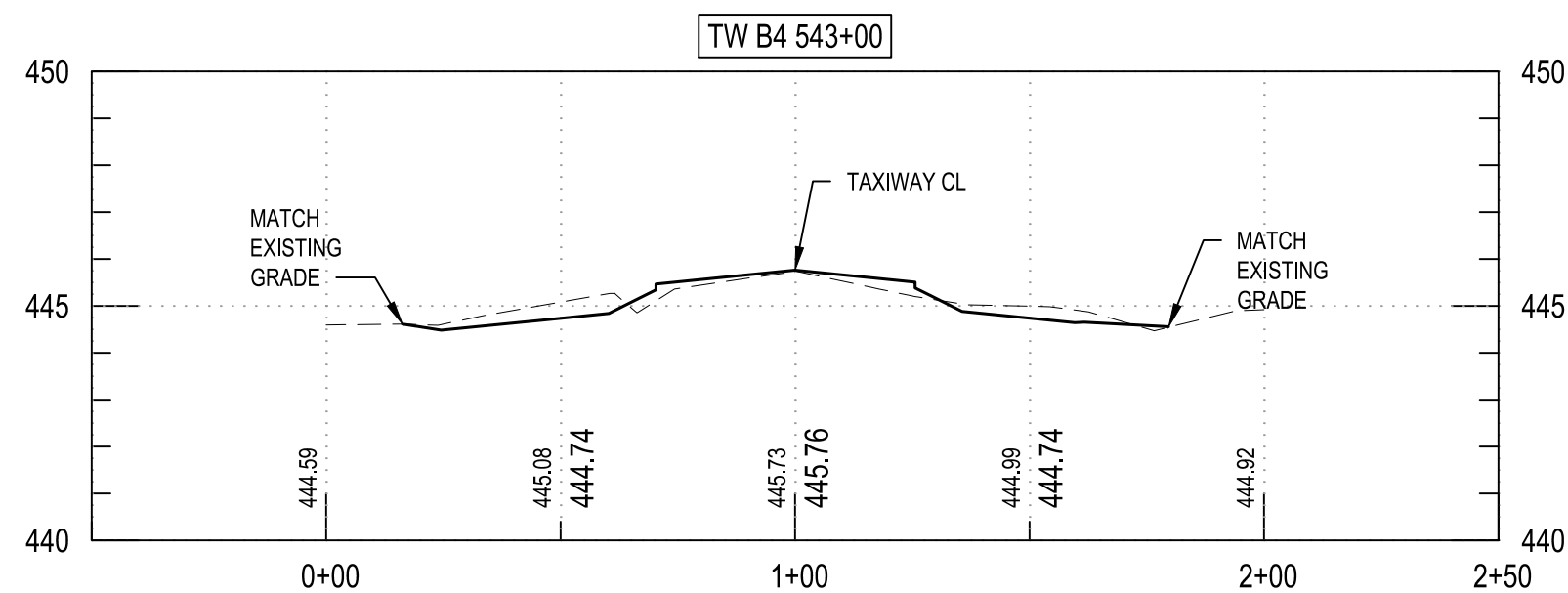
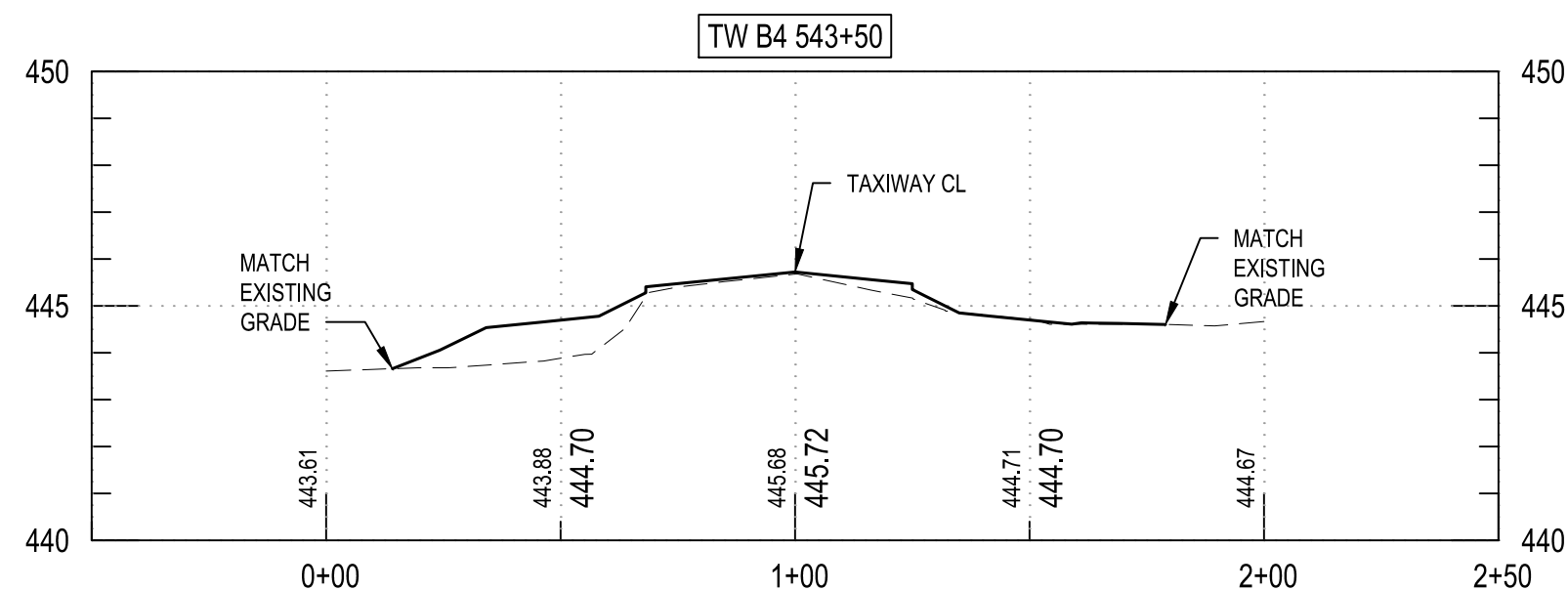
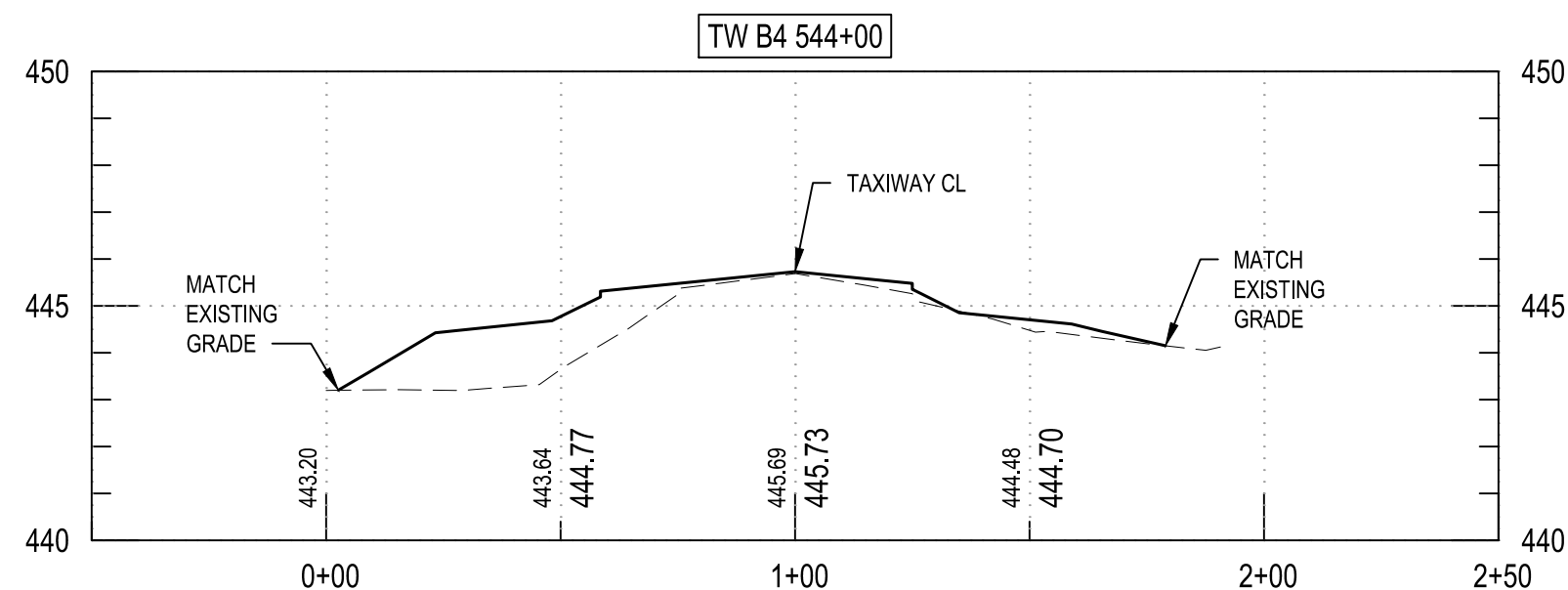
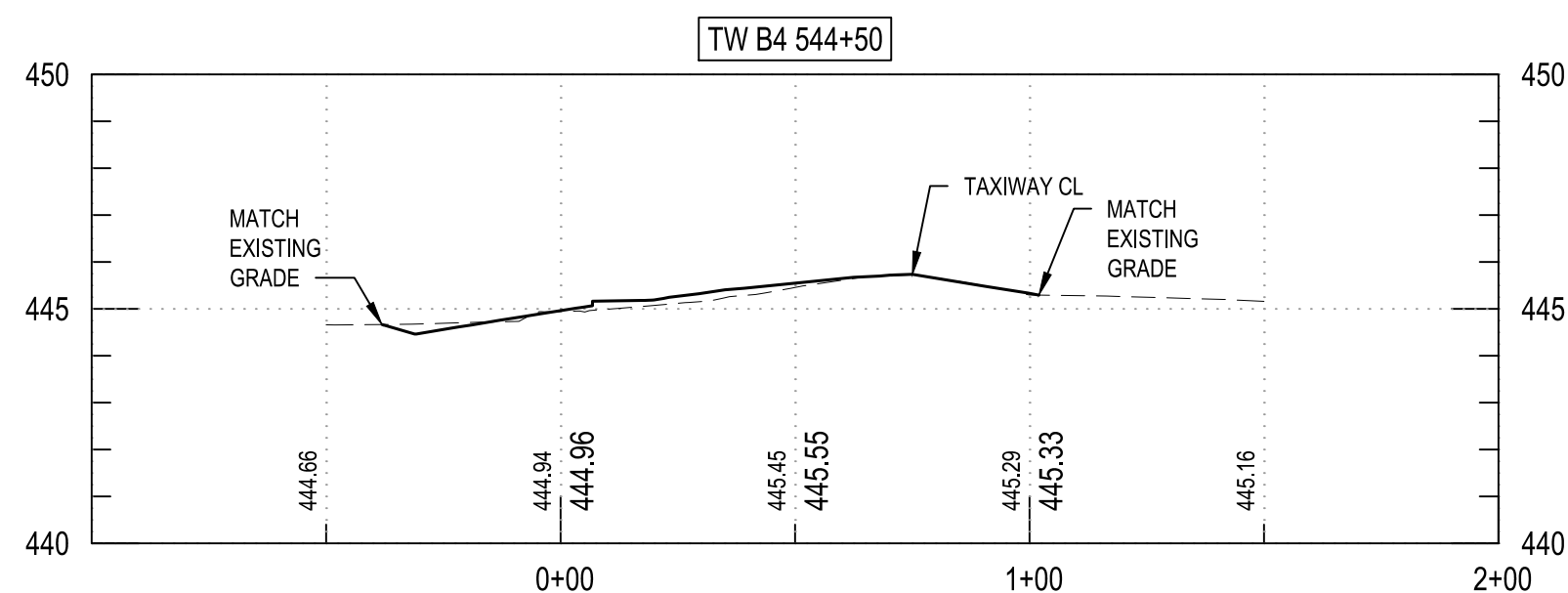
IMPERIAL COUNTY AIRPORT
PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5

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CHECKED BY:	S.L. UNDERWOOD	

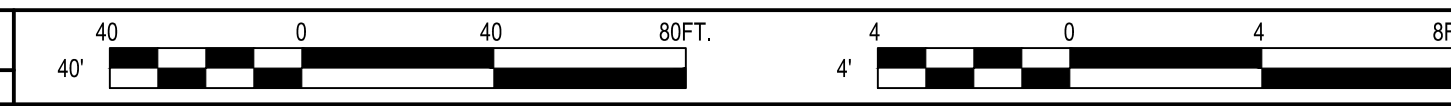
PROFILE

CG201
 11 of 21

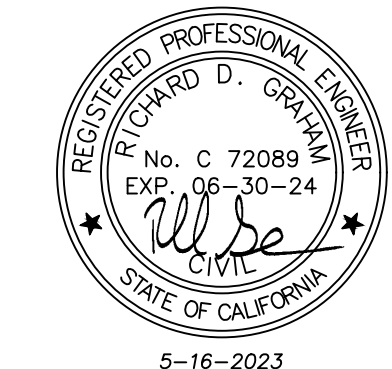
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A1 CROSS SECTIONS
 SCALE: 1" = 40' HORIZ, 1" = 4' VERT.



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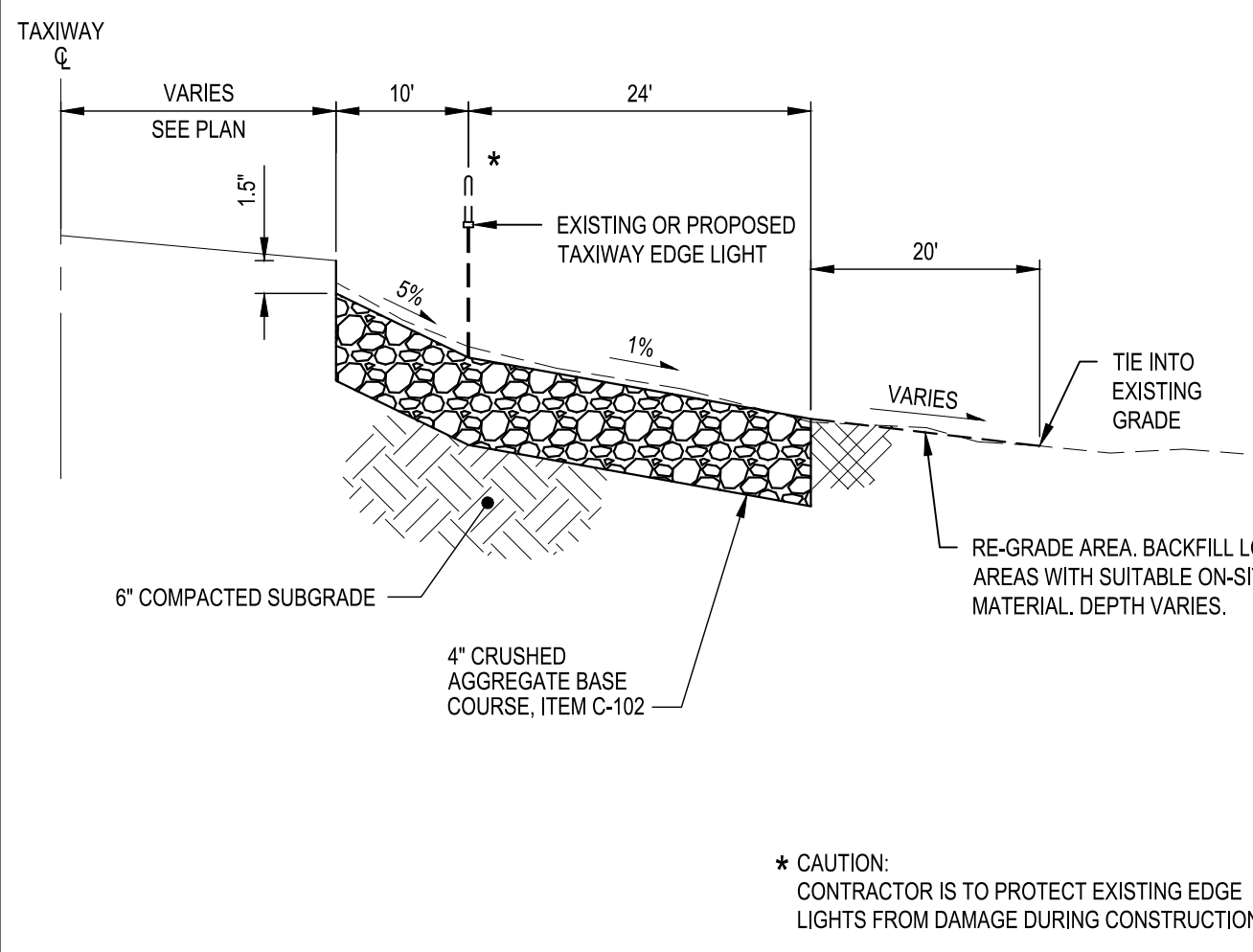


IMPERIAL COUNTY AIRPORT
**PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5**

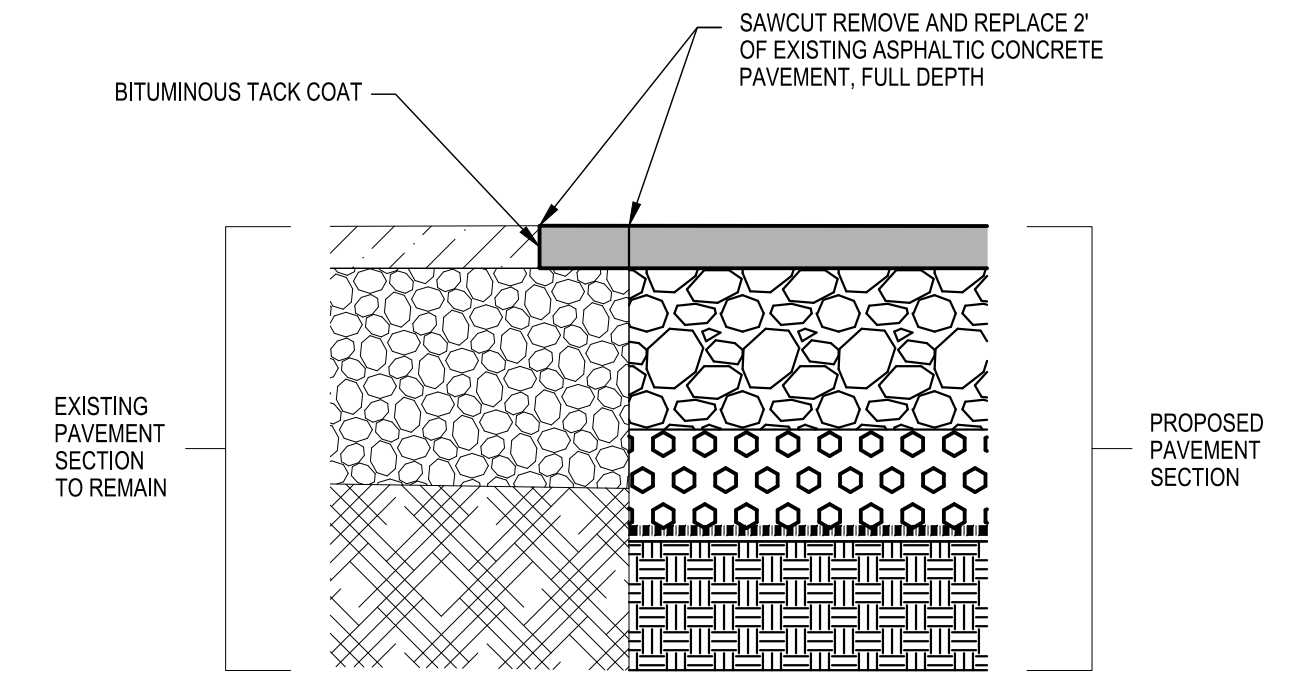
MARK	DATE	DESCRIPTION
REVISIONS		
		PROJECT NO: K3002019
		DATE: MAY 2023
		DRAWN BY: G.C. HAYDEN
		DESIGNED BY: M.E. BARR
		CHECKED BY: S.L. UNDERWOOD

CROSS SECTIONS

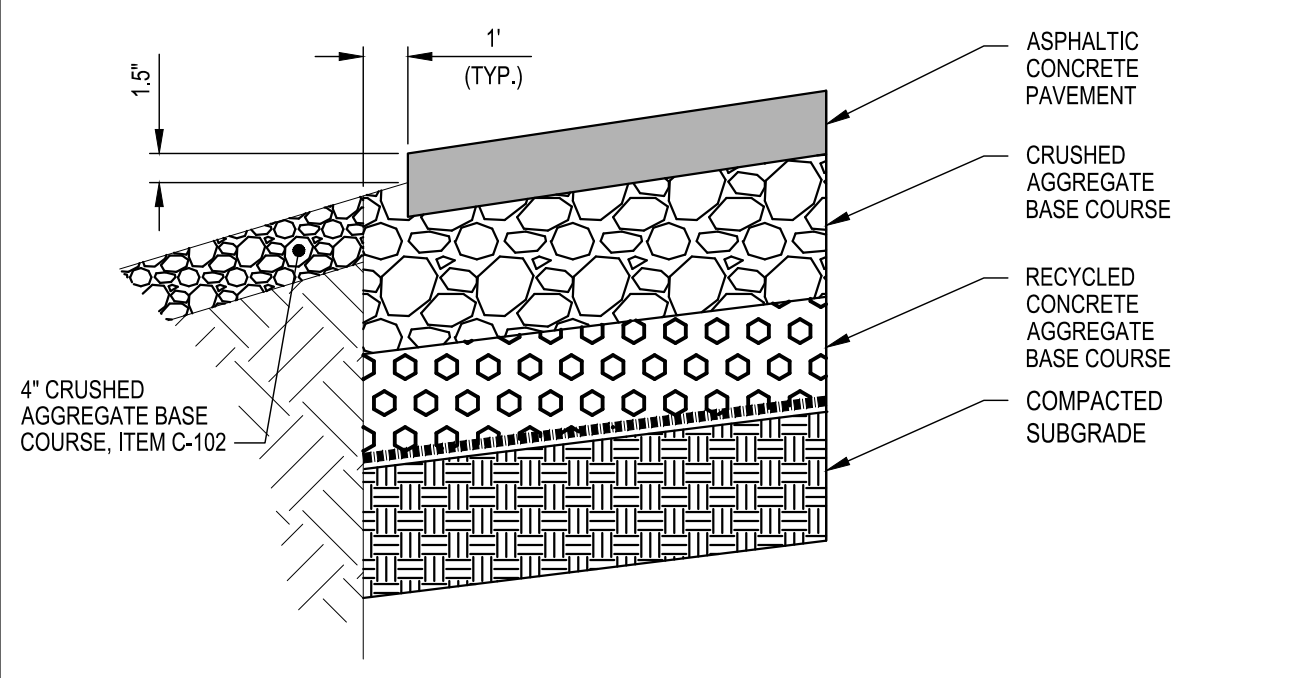
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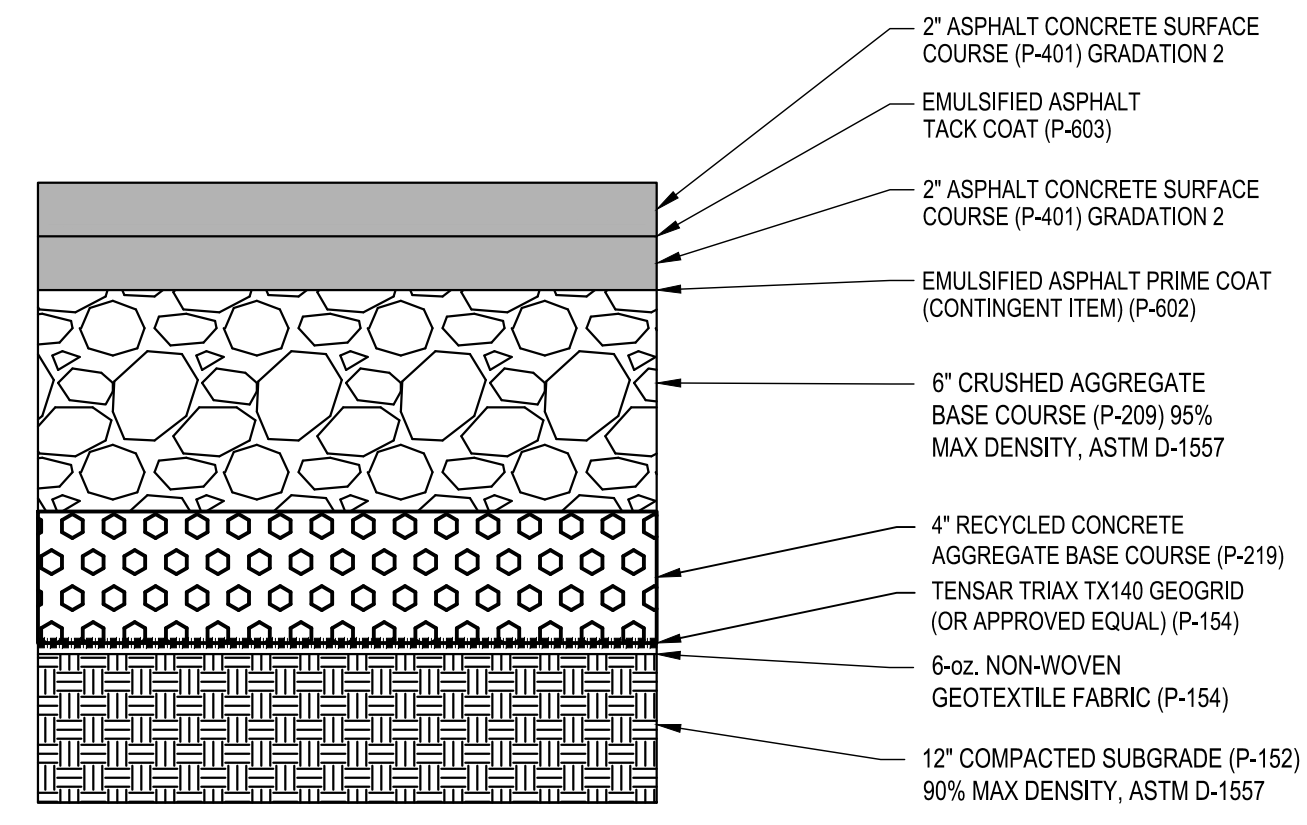
C1 CRUSHED AGGREGATE INFIELD PROTECTION DETAIL
SCALE: NOT TO SCALE



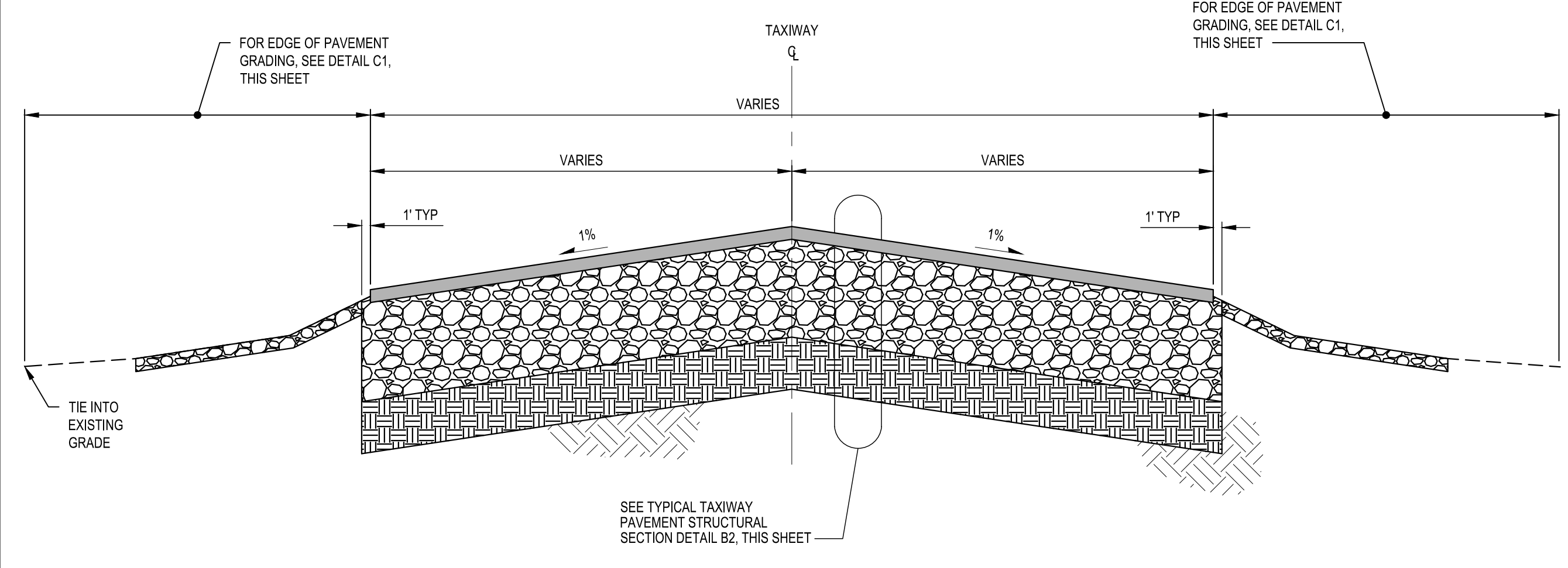
C2 TYPICAL MATCH EXISTING PAVEMENT DETAIL
SCALE: NOT TO SCALE



B1 TYPICAL PAVEMENT EDGE DETAIL
SCALE: NOT TO SCALE



B2 TYPICAL TAXIWAY PAVEMENT STRUCTURAL SECTION
SCALE: NOT TO SCALE



A1 TYPICAL TAXIWAY SECTION - SAFETY AREA IMPROVEMENTS
SCALE: NOT TO SCALE



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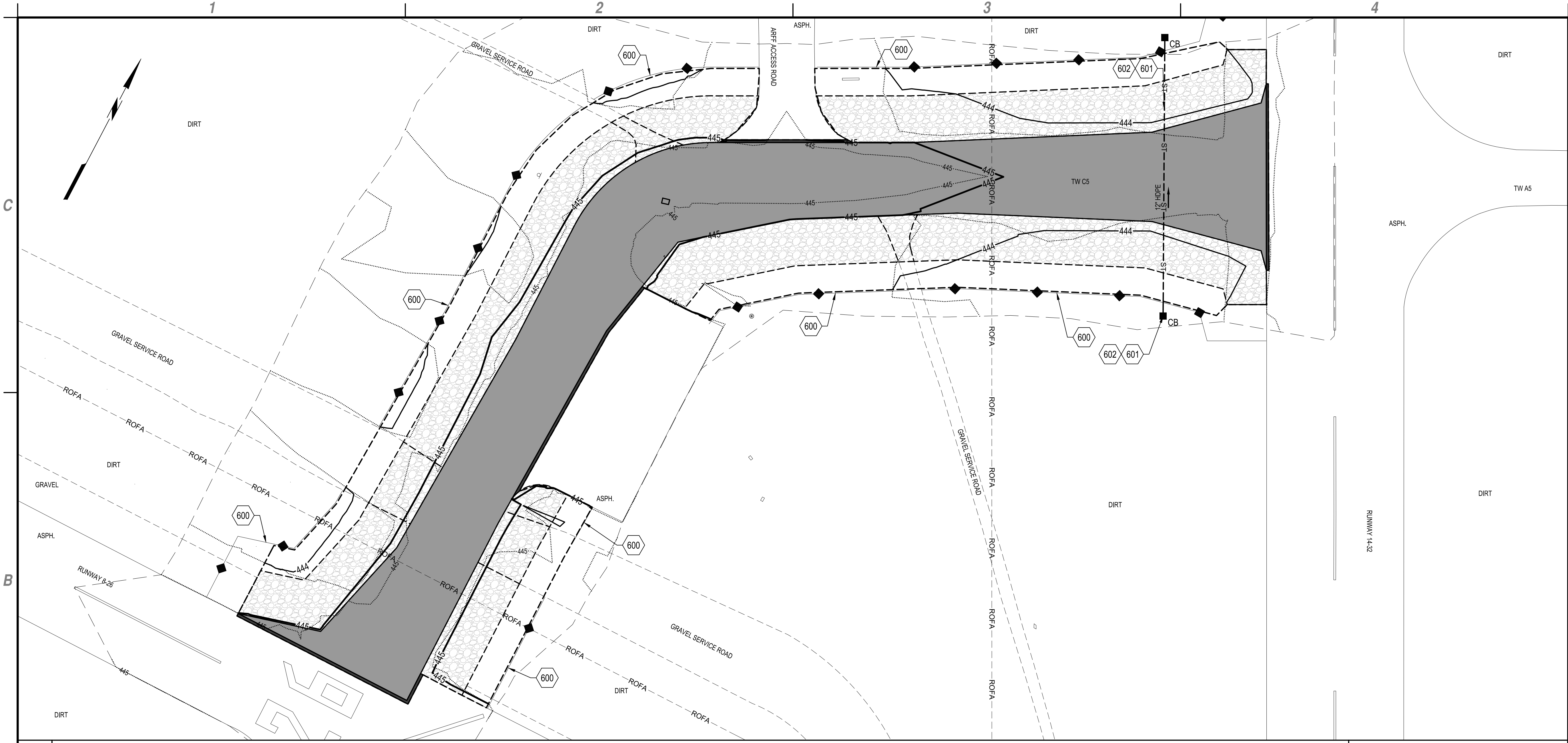
IMPERIAL COUNTY AIRPORT
PAVEMENT REHABILITATION
(APMS 1 & 2) TAXIWAY B4/C5

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TYPICAL SECTIONS

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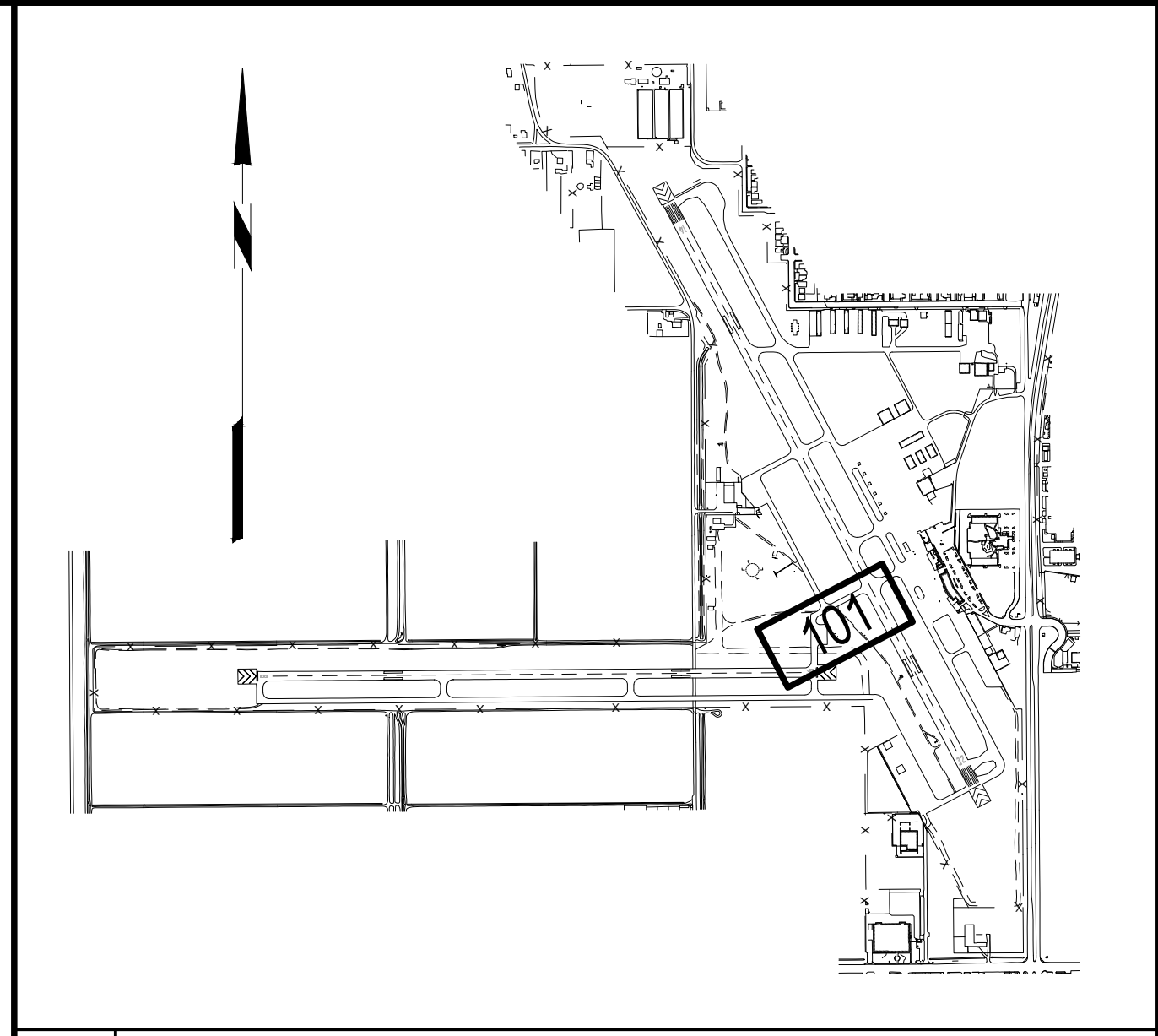
August 3, 2022 - 2:58PM
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B1 SOIL EROSION AND SEDIMENT CONTROL PLAN
 SCALE: 1" = 40'



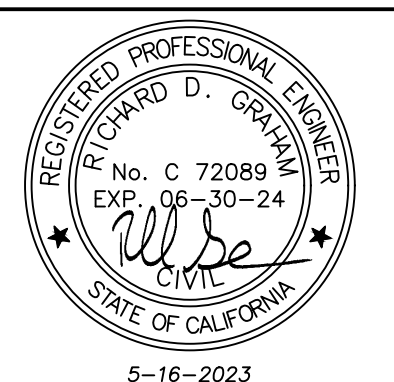
- A1** KEYED NOTES
 SCALE: NOT TO SCALE
- 600. SILT FENCE, ITEM C-102. SEE DETAIL A1/CE501.
 - 601. STORM DRAIN INLET PROTECTION, ITEM C-102. SEE DETAIL A4/CE501.
 - 602. INSTALL INLET FILTER INSERT, ITEM C-102. SEE DETAIL B1/CE501.



A4 KEY MAP
 SCALE: NOT TO SCALE



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IMPERIAL CALIFORNIA
 IMPERIAL COUNTY AIRPORT
 PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5

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SOIL EROSION AND SEDIMENT CONTROL PLAN

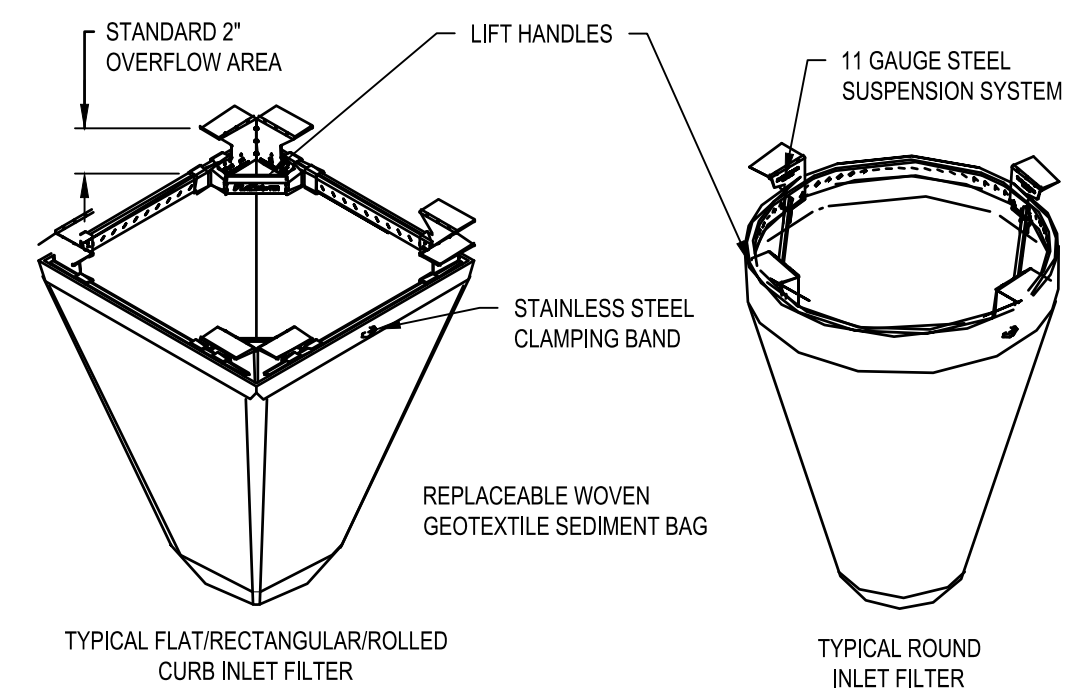
CE101
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1

2

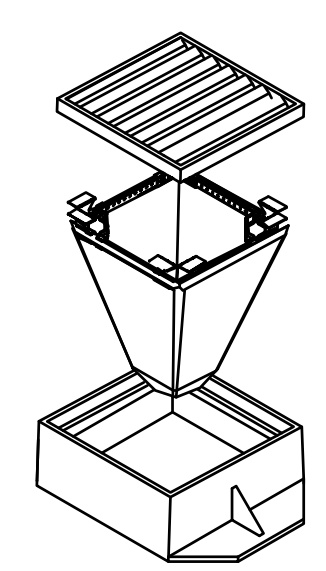
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4



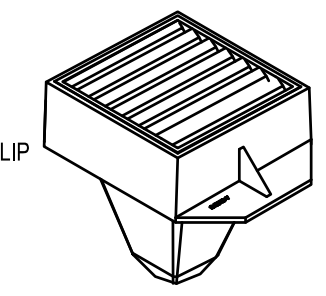
MATERIALS:

- FRAMING - 11 GAUGE STEEL; CORROSION RESISTANT.
- SEDIMENT BAG - WOVEN GEOTEXTILE (TYPE FF OR APPROVED ALTERNATIVE); 2-CUBIC FT TYP VOLUME, STAINLESS STEEL LOCKING BAND SECURING BAG TO FRAME.



INSTALLATION:

- REMOVE GRATE.
- DROP MANUFACTURED INLET FILTER ONTO LOAD BEARING LIP OF CASTING OR CONCRETE STRUCTURE.
- REPLACE GRATE.



BASIS OF DESIGN:

FLEXSTORM BY ADS WITH WOVEN STANDARD BAG.

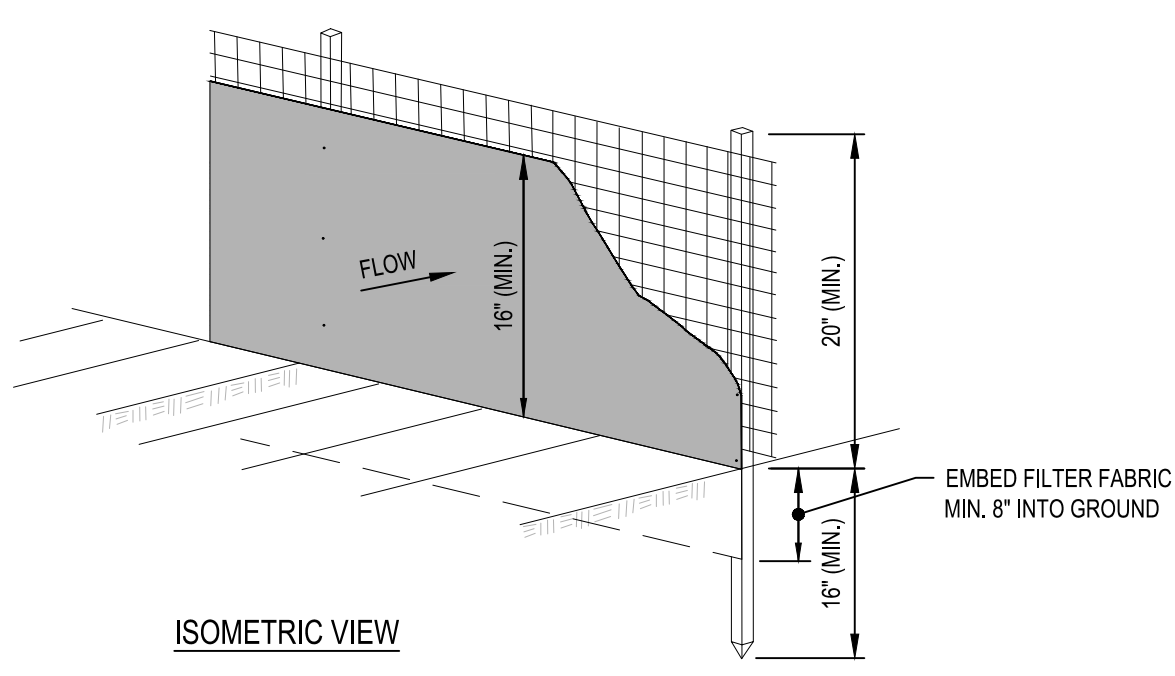
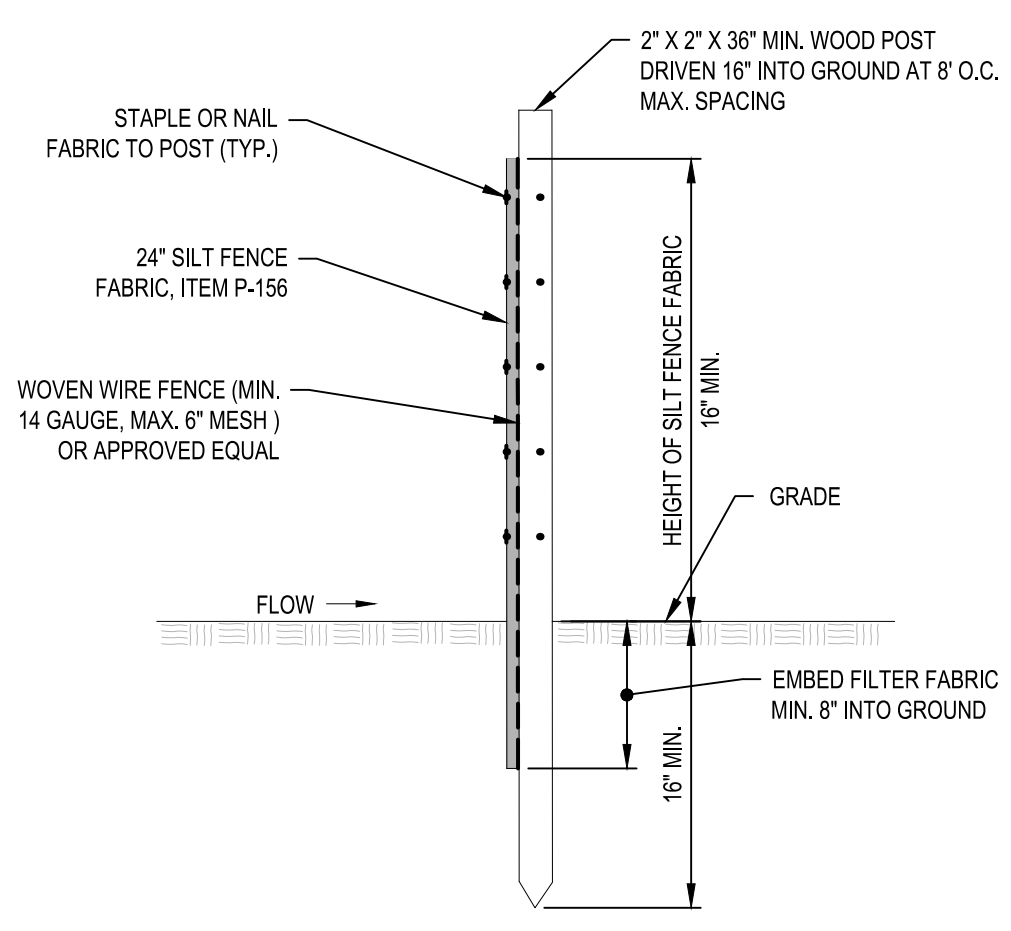
OTHER ACCEPTABLE MANUFACTURERS:

GRATE INLET SKIMMER BOX BY SUNTREE TECHNOLOGIES.
 GRATE INLET SKIMMER BOX BY BIO CLEAN ENVIRONMENTAL SERVICES OR APPROVED EQUAL.

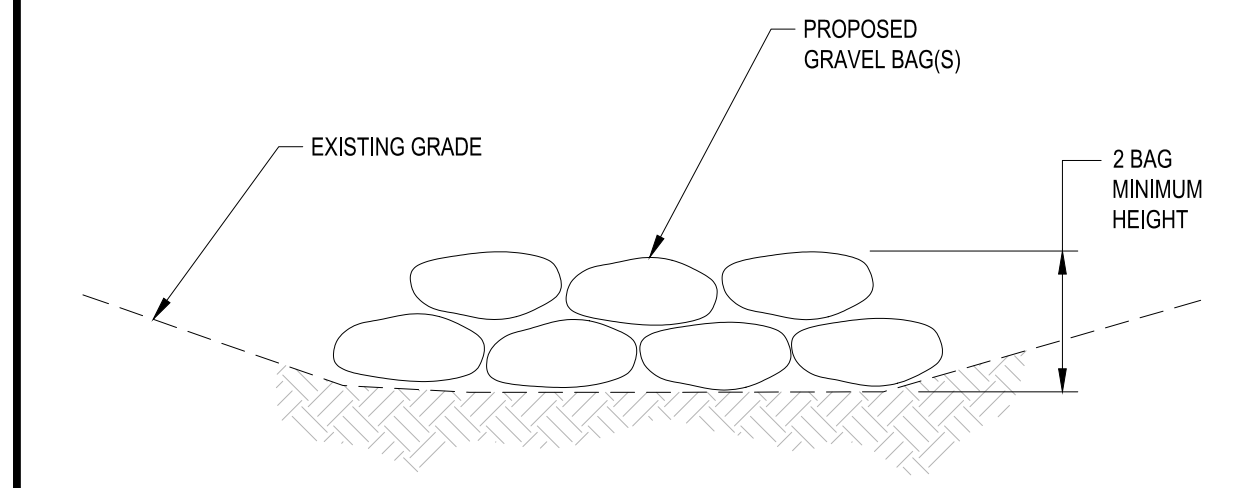
B1 INLET FILTER INSERT (IP)
 SCALE: NOT TO SCALE

SILT FENCE NOTES:

- WHEN TWO SECTIONS OF SILT FENCE FABRIC ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" AND FOLDED.
- MAINTENANCE SHALL BE PROVIDED BY THE CONTRACTOR AS DIRECTED BY ENGINEER AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE.
- WOVEN WIRE FENCE SHALL BE SECURELY FASTENED TO FENCE POSTS WITH WIRE TIES OR STAPLES.
- SILT FENCE FABRIC SHALL BE SECURELY FASTENED TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 24" AT TOP AND MID-SECTION.
- CONTRACTOR SHALL REMOVE THE SILT FENCE AT THE APPROPRIATE TIME, DRESS THE DISTURBED AREAS, AND DISPOSE OF THE SILT FENCE.



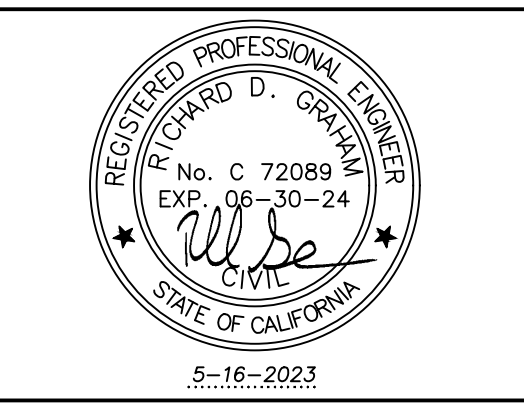
A1 SILT FENCE DETAIL (SF)
 SCALE: 1:250



A4 STORM DRAIN INLET DETAIL (SD)
 SCALE: NOT TO SCALE



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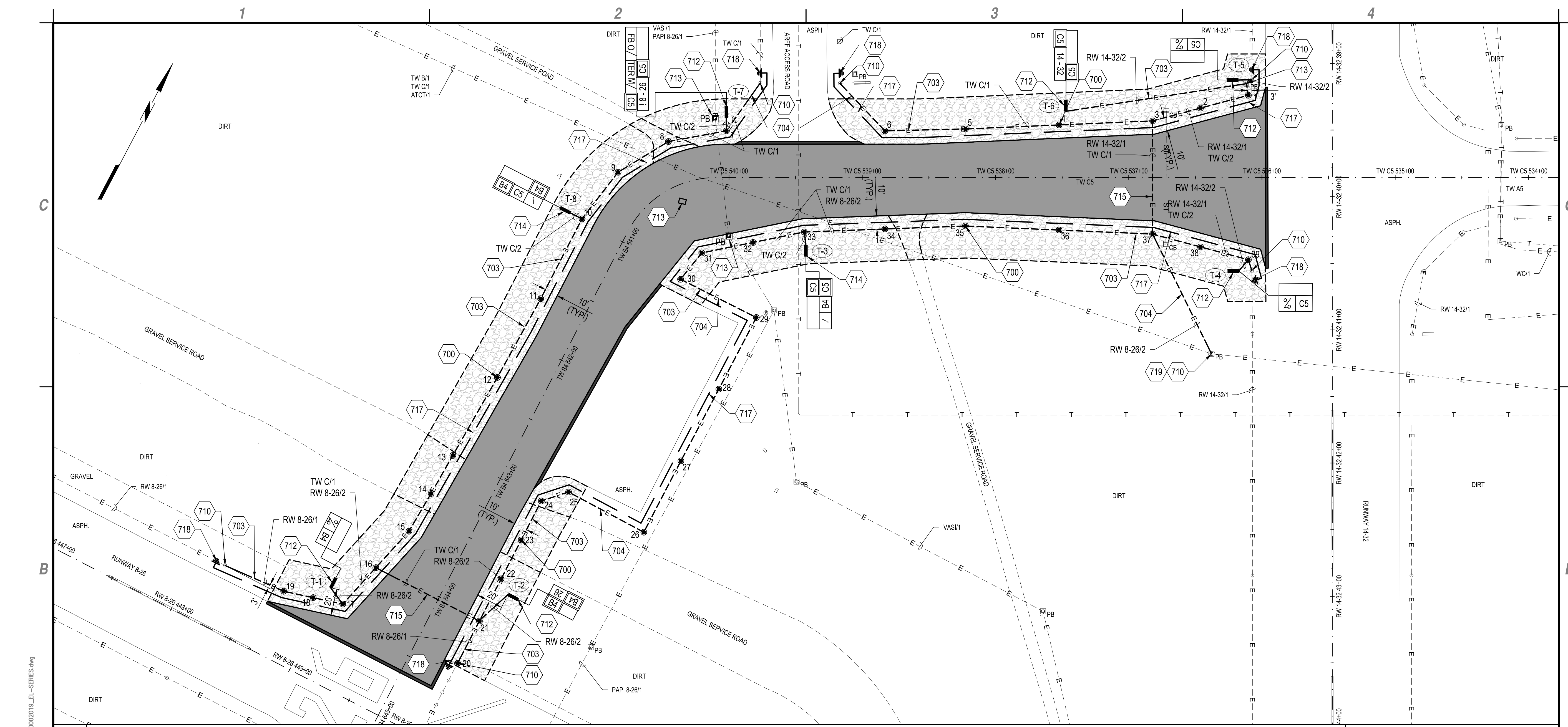
IMPERIAL COUNTY AIRPORT
 PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5

MARK	DATE	DESCRIPTION
REVISIONS		
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DATE:	MAY 2023	
DRAWN BY:	G.C. HAYDEN	
DESIGNED BY:	M.E. BARR	
CHECKED BY:	S.L. UNDERWOOD	

SOIL EROSION AND SEDIMENT CONTROL DETAILS

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August 3, 2022 4:25:58PM F:\Project\K30 - Imperial County, CA\K30002019 - Pavement Rehabilitation (Design)\Design\CADD (Package 2)\Sheet Files\K30002019_CE-SERIES.dwg



B1 AIRFIELD LIGHTING AND SIGNAGE PLAN
SCALE: 1" = 40'



- A1 KEYED NOTES**
- 700. ELEVATED BASE MOUNTED L-861T TAXIWAY EDGE LIGHT, ITEM L-125 (TYP.). SEE DETAIL B3/EL501
 - 703. NO. 8 AWG, 5KV, 1/C AIRFIELD LIGHTING CABLE(S), ITEM L-108, IN 2" PVC CONDUIT IN GRAVEL, ITEM L-110. SEE CONDUIT TRENCHING DETAIL A3/EL501.
 - 704. NO. 8 AWG, 5KV, 1/C AIRFIELD LIGHTING CABLE(S), ITEM L-108, IN 2" PVC CONDUIT IN DIRT, ITEM L-110. SEE CONDUIT TRENCHING DETAIL A3/EL501
 - 710. CONNECT TO EXISTING CIRCUIT IN EXISTING LIGHT BASE OR PULL BOX
 - 712. RELOCATED GUIDANCE SIGN ON NEW FOUNDATION, ITEM L-125. SEE DETAIL C3/EL501
 - 713. ELECTRICAL PULLBOX TO BE MODIFIED, ITEM L-115. SEE DETAIL B1/EL502
 - 714. PROPOSED GUIDANCE SIGN, ITEM L-125. SEE DETAIL A1/EL501
 - 715. NO. 8 AWG, 5KV, 1/C AIRFIELD LIGHTING CABLE(S), ITEM L-108, IN 2" PVC CONDUIT IN ASPHALT PAVEMENT, ITEM L-110. SEE CONDUIT TRENCHING DETAIL A3/EL501
 - 717. COUNTERPOISE WIRE AND TRENCH, ITEM L-108. SEE DETAIL A1/EL502
 - 718. CONNECT PROPOSED COUNTERPOISE WIRE TO EXISTING COUNTERPOISE WIRE
 - 719. CONTRACTOR TO VERIFY EXISTING CIRCUITING

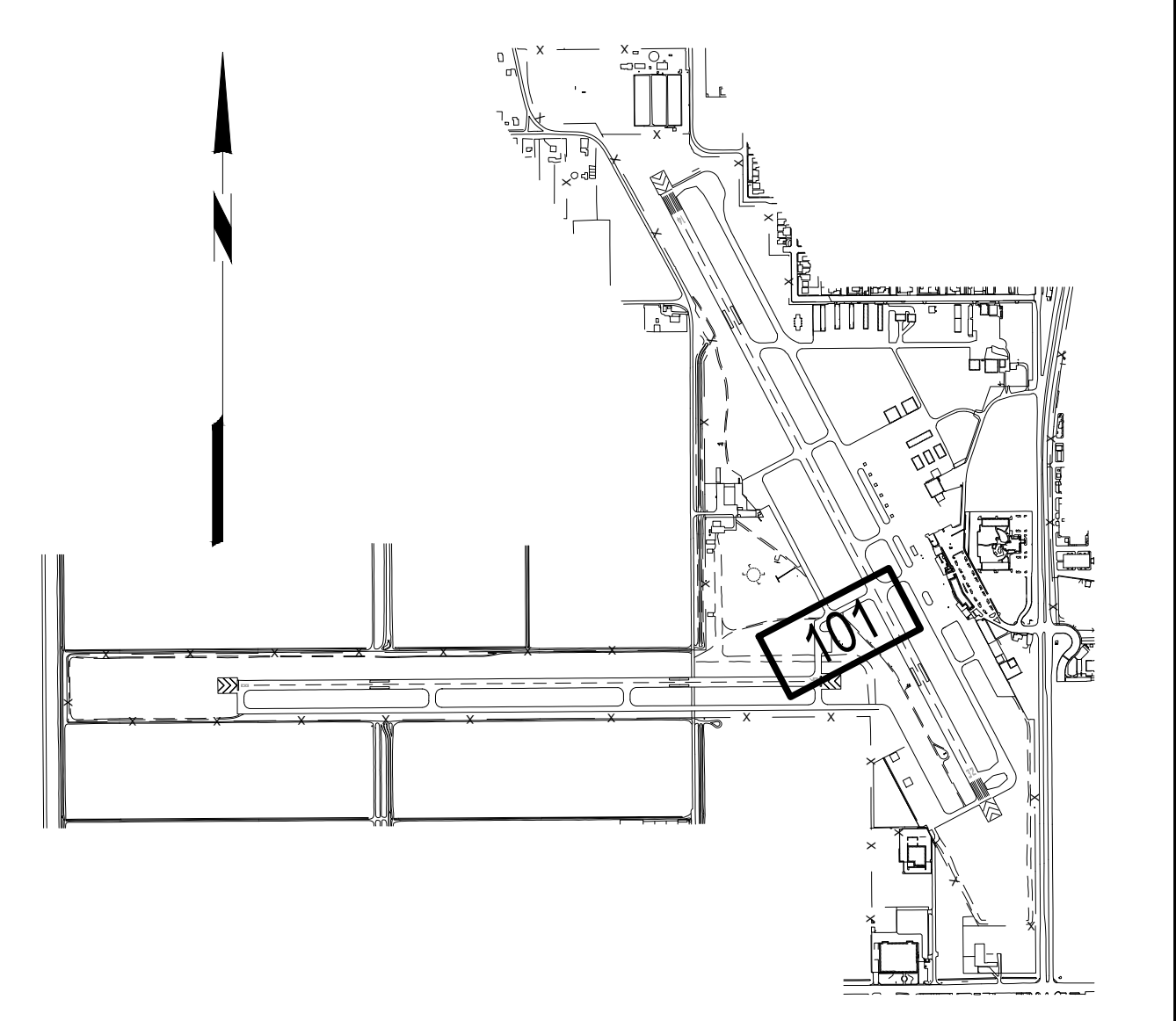
A2 EDGE LIGHT TABLE
SCALE: NOT TO SCALE

EDGE LIGHT TABLE			EDGE LIGHT TABLE			EDGE LIGHT TABLE		
NUMBER	NORTHING	EASTING	NUMBER	NORTHING	EASTING	NUMBER	NORTHING	EASTING
1	1,883,926.67	6,769,204.29	14	1,883,376.99	6,768,799.95	27	1,883,485.76	6,768,954.64
2	1,883,901.44	6,769,176.93	15	1,883,344.11	6,768,798.42	28	1,883,546.66	6,768,954.82
3	1,883,876.22	6,769,149.58	16	1,883,308.36	6,768,789.11	29	1,883,607.33	6,768,954.74
4	1,883,840.92	6,769,088.80	17	1,883,272.62	6,768,779.80	30	1,883,806.37	6,768,891.06
5	1,883,805.62	6,769,028.02	18	1,883,266.60	6,768,757.96	31	1,883,631.24	6,768,895.97
6	1,883,776.25	6,768,975.11	19	1,883,260.59	6,768,736.11	32	1,883,656.10	6,768,926.40
7	1,883,721.00	6,768,869.77	20	1,883,273.29	6,768,876.80	33	1,883,680.95	6,768,956.83
8	1,883,693.66	6,768,834.95	21	1,883,308.99	6,768,876.56	34	1,883,711.02	6,769,009.33
9	1,883,655.70	6,768,812.18	22	1,883,344.65	6,768,876.31	35	1,883,741.09	6,769,061.84
10	1,883,612.11	6,768,804.45	23	1,883,377.52	6,768,876.08	36	1,883,771.04	6,769,125.44
11	1,883,544.66	6,768,804.92	24	1,883,410.39	6,768,875.80	37	1,883,800.98	6,769,189.04
12	1,883,477.25	6,768,803.57	25	1,883,425.85	6,768,890.81	38	1,883,809.15	6,769,225.35
13	1,883,409.87	6,768,801.14	26	1,883,425.40	6,768,954.84	39	1,883,817.32	6,769,261.65

A3 GUIDANCE SIGN INDEX
SCALE: NOT TO SCALE

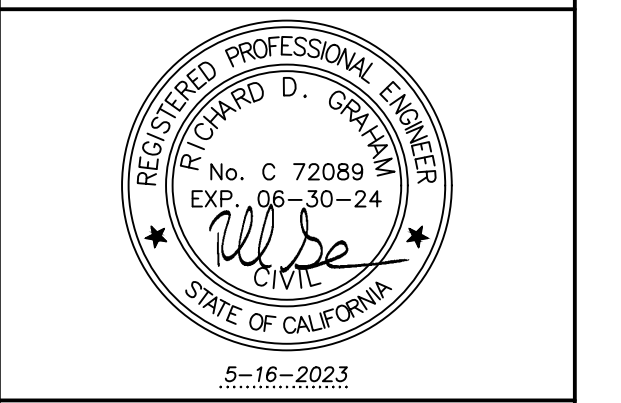
SIGN NO.	NO. OF CHARACTERS	DESIGNATION	TYPE	SIDE 1	SIDE 2
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		TYPE		Y	
T-2	2	DESIGNATION		B4 2 6	B4
		TYPE		L R	L
T-3	3	DESIGNATION		← B4 C5	C5
		TYPE		Y L	L
T-4	2	DESIGNATION		← C5	
		TYPE		Y	
T-5	2	DESIGNATION		C5 →	
		TYPE		Y	
T-6	3	DESIGNATION		C5 1 4 - 3 2	C5
		TYPE		L R	L
T-7	5	DESIGNATION		FBO ↑ TERM ↑ C5	← 8-26 C5
		TYPE		Y Y L	Y L
T-8	3	DESIGNATION		B4 C5 →	B4
		TYPE		L Y	L

R - INDICATES L-858R MANDATORY INSTRUCTION SIGN (WHITE INSCRIPTION ON A RED BACKGROUND)
L - INDICATES L-858L LOCATION SIGN (YELLOW INSCRIPTION ON A BLACK BACKGROUND)
Y - INDICATES L-858Y DIRECTION SIGN (BLACK INSCRIPTION ON A YELLOW BACKGROUND)
■ - INDICATES BLANK PANEL OR BLANK AREA OF PANEL



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IMPERIAL COUNTY AIRPORT
PAVEMENT REHABILITATION (APMS 1 & 2) TAXIWAY B4/C5

MARK DATE DESCRIPTION

REVISIONS

MARK	DATE	DESCRIPTION

PROJECT NO: K30002019
DATE: MAY 2023
DRAWN BY: G.C. HAYDEN
DESIGNED BY: M.E. BARR
CHECKED BY: S.L. UNDERWOOD

AIRFIELD LIGHTING AND SIGNAGE PLAN

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16 of 21

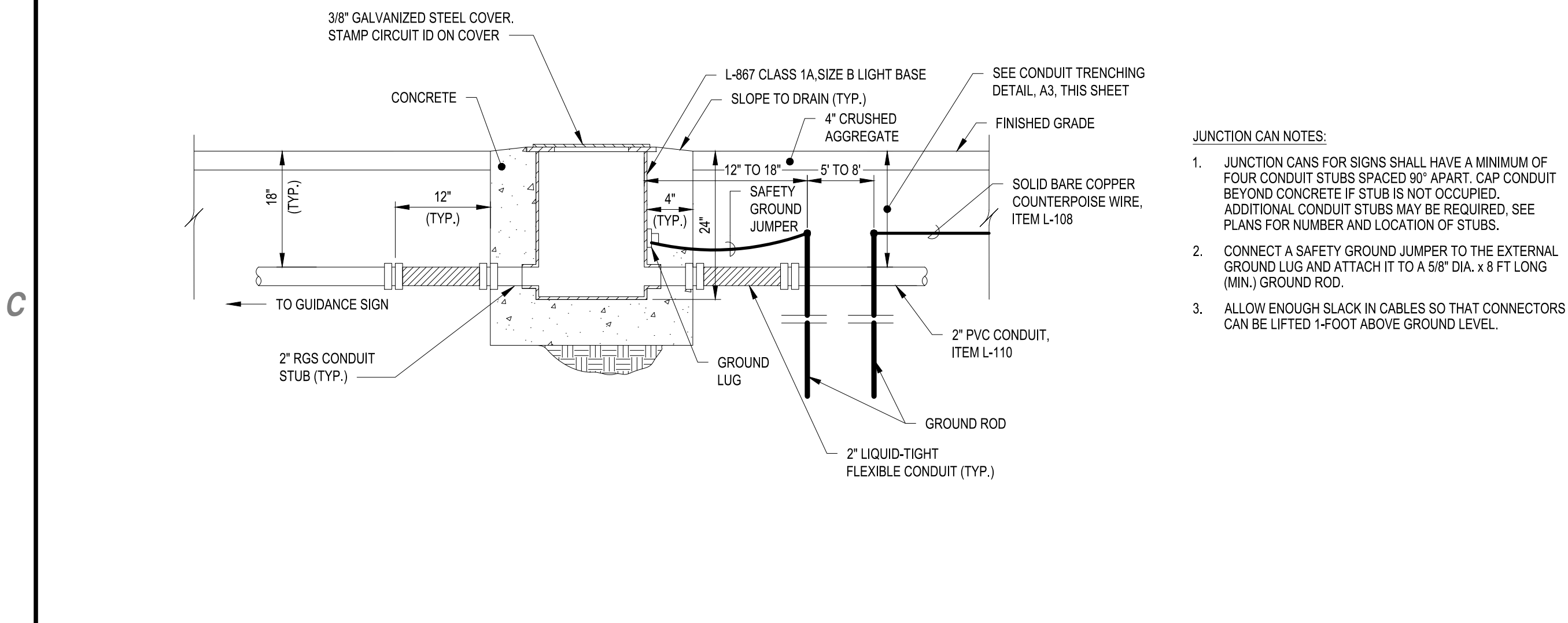
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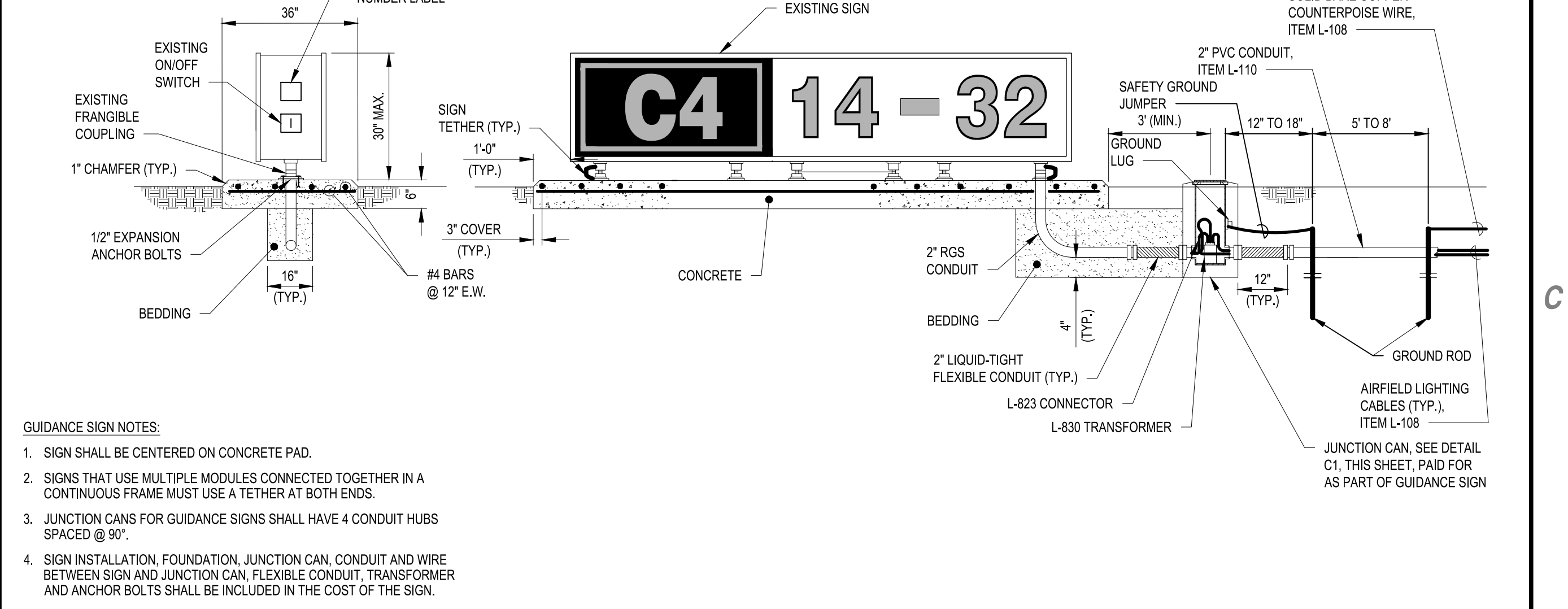
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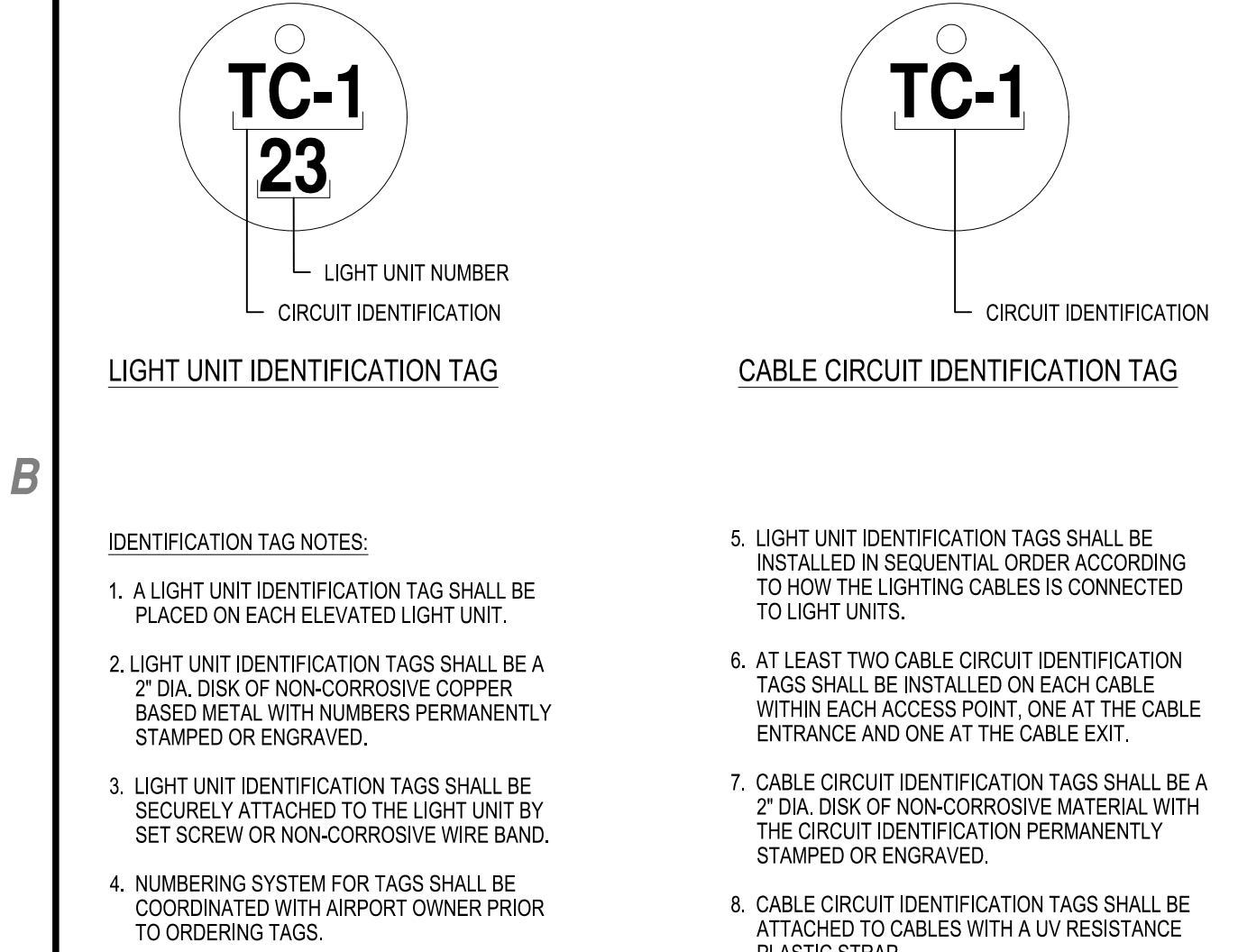
- JUNCTION CAN NOTES:**
1. JUNCTION CANS FOR SIGNS SHALL HAVE A MINIMUM OF FOUR CONDUIT STUBS SPACED 90° APART. CAP CONDUIT BEYOND CONCRETE IF STUB IS NOT OCCUPIED. ADDITIONAL CONDUIT STUBS MAY BE REQUIRED, SEE PLANS FOR NUMBER AND LOCATION OF STUBS.
 2. CONNECT A SAFETY GROUND JUMPER TO THE EXTERNAL GROUND LUG AND ATTACH IT TO A 5/8" DIA. x 8 FT LONG (MIN.) GROUND ROD.
 3. ALLOW ENOUGH SLACK IN CABLES SO THAT CONNECTORS CAN BE LIFTED 1-FOOT ABOVE GROUND LEVEL.

C1 JUNCTION CAN DETAIL
SCALE: NOT TO SCALE

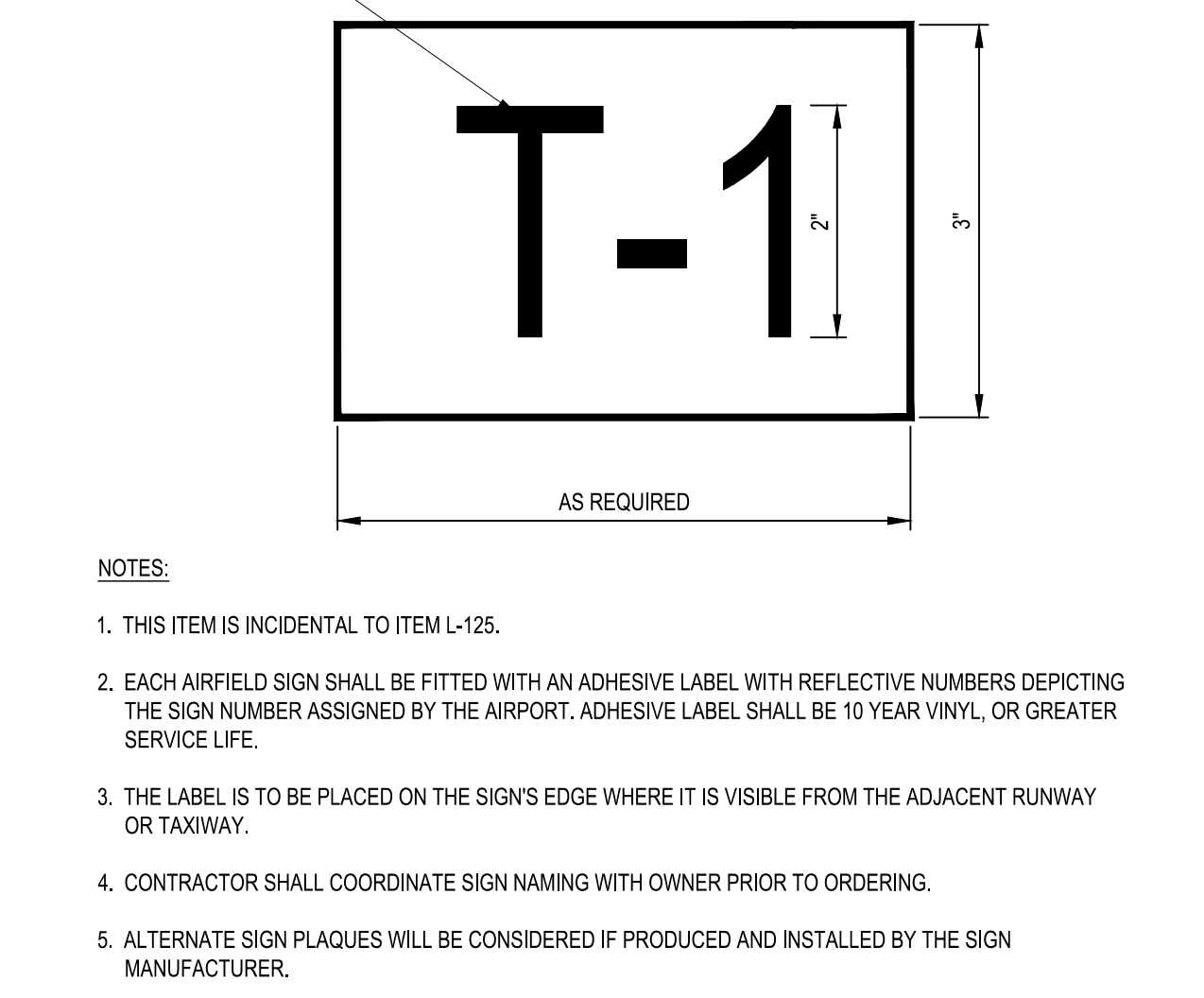


- GUIDANCE SIGN NOTES:**
1. SIGN SHALL BE CENTERED ON CONCRETE PAD.
 2. SIGNS THAT USE MULTIPLE MODULES CONNECTED TOGETHER IN A CONTINUOUS FRAME MUST USE A TETHER AT BOTH ENDS.
 3. JUNCTION CANS FOR GUIDANCE SIGNS SHALL HAVE 4 CONDUIT HUBS SPACED @ 90°.
 4. SIGN INSTALLATION, FOUNDATION, JUNCTION CAN, CONDUIT AND WIRE BETWEEN SIGN AND JUNCTION CAN, FLEXIBLE CONDUIT, TRANSFORMER AND ANCHOR BOLTS SHALL BE INCLUDED IN THE COST OF THE SIGN.

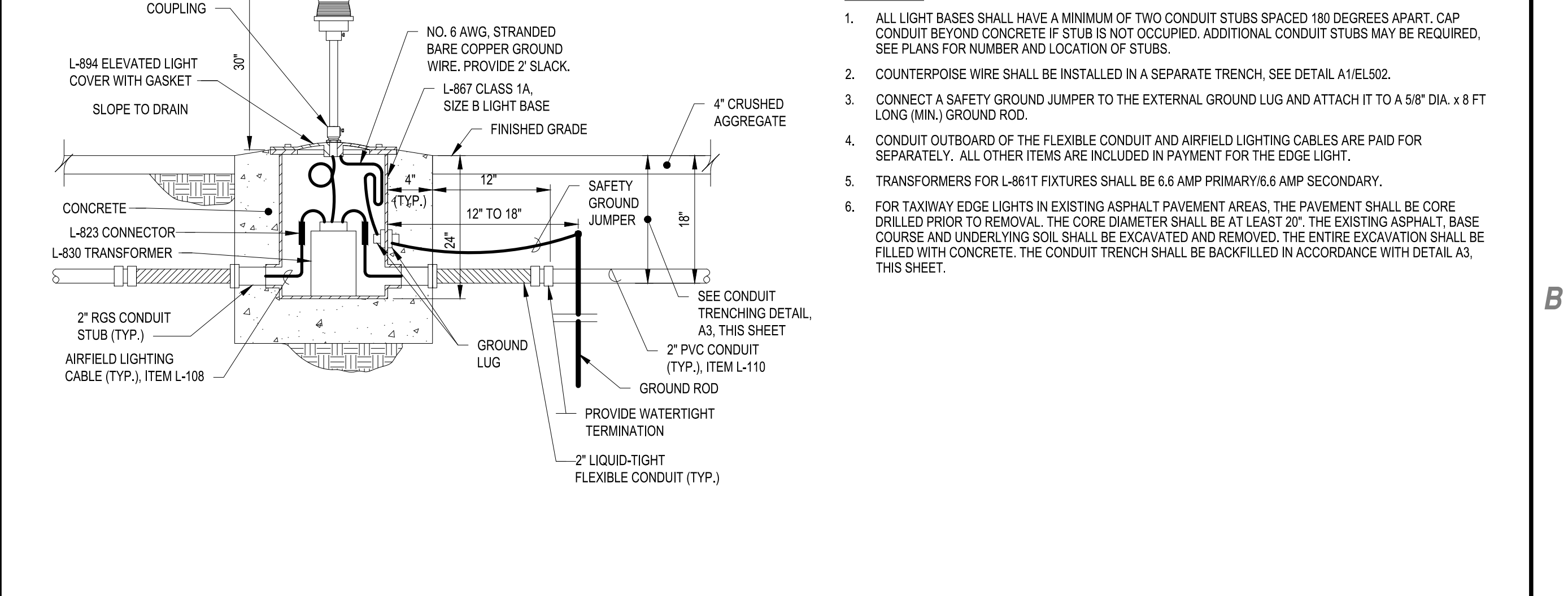
C3 RELOCATED GUIDANCE SIGN DETAIL
SCALE: NOT TO SCALE



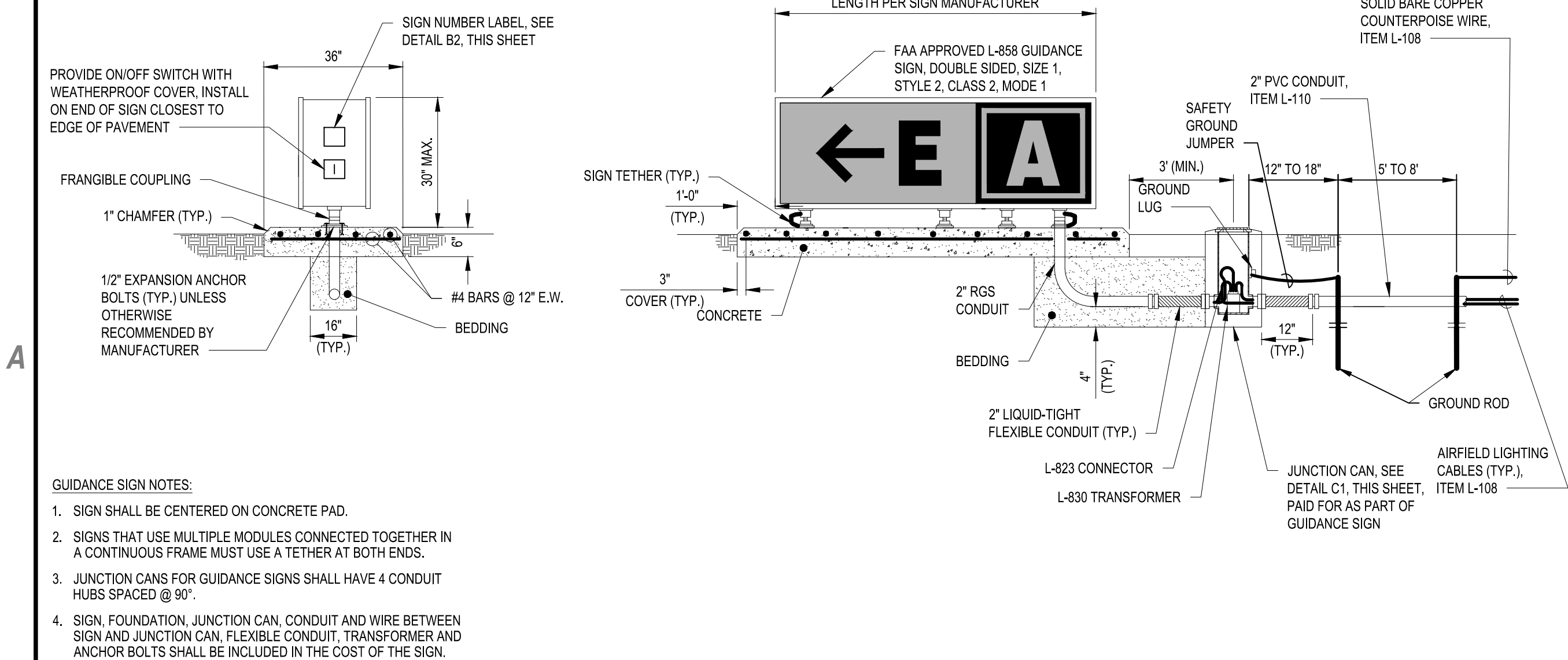
B1 IDENTIFICATION TAG DETAIL
SCALE: NOT TO SCALE



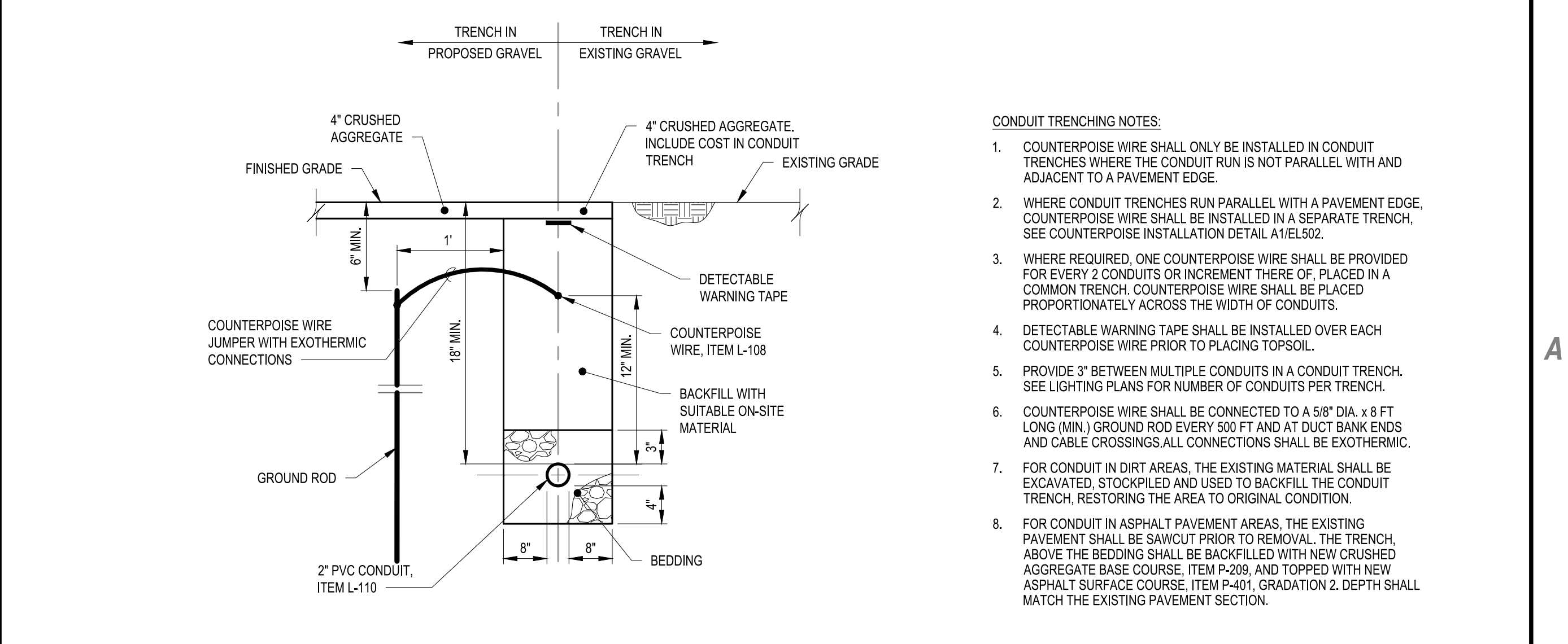
B2 SIGN IDENTIFICATION DETAIL
SCALE: NOT TO SCALE



B3 ELEVATED LIGHT IN GRAVEL DETAIL (BASE MOUNTED)
SCALE: NOT TO SCALE



A1 PROPOSED GUIDANCE SIGN DETAIL
SCALE: NOT TO SCALE



A3 CONDUIT TRENCHING DETAIL (IN GRAVEL OR DIRT)
SCALE: NOT TO SCALE



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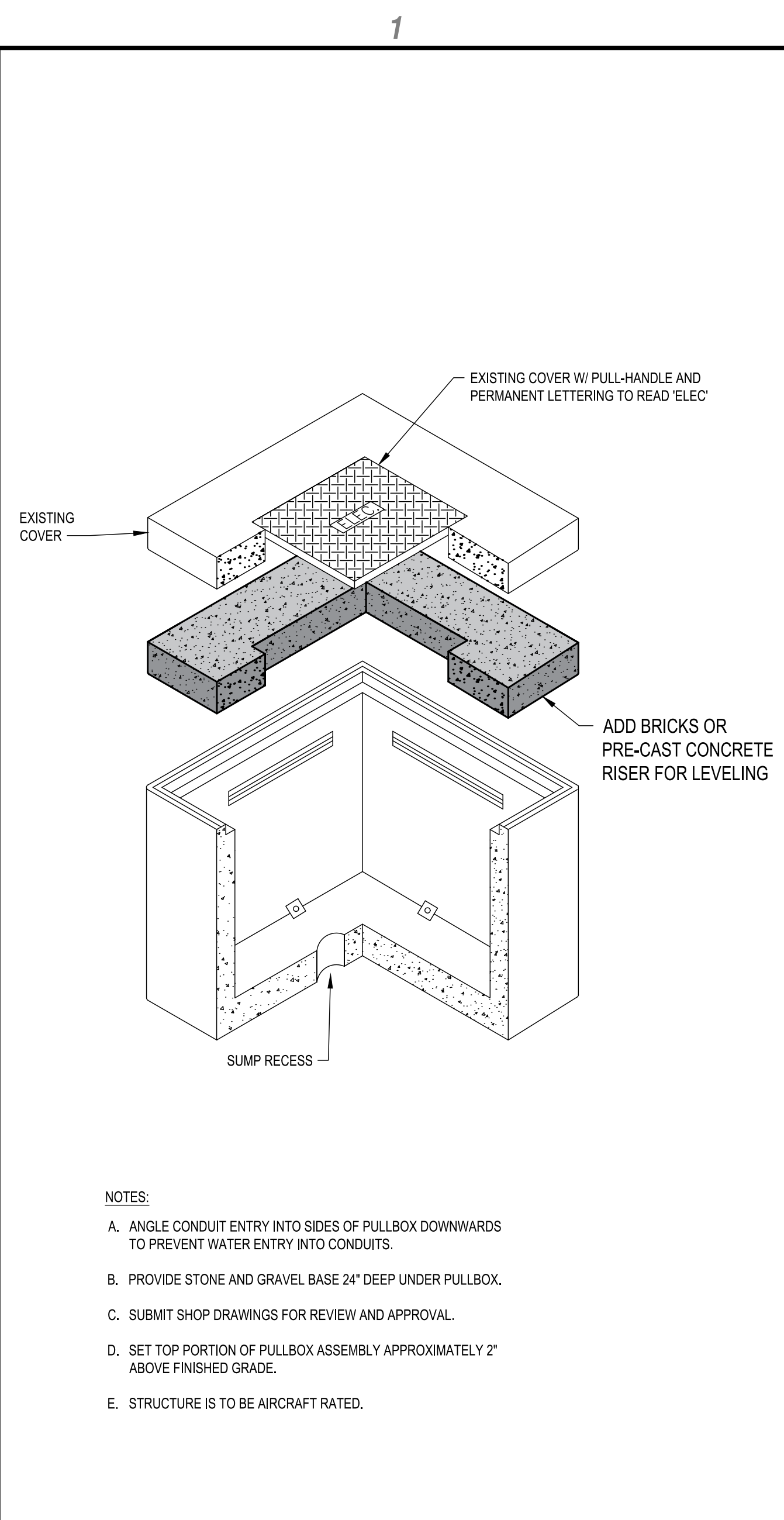
IMPERIAL COUNTY AIRPORT
PAVEMENT REHABILITATION
(APMS 1 & 2) TAXWAY B4/C5

MARK	DATE	DESCRIPTION
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AIRFIELD LIGHTING AND SIGNAGE DETAILS

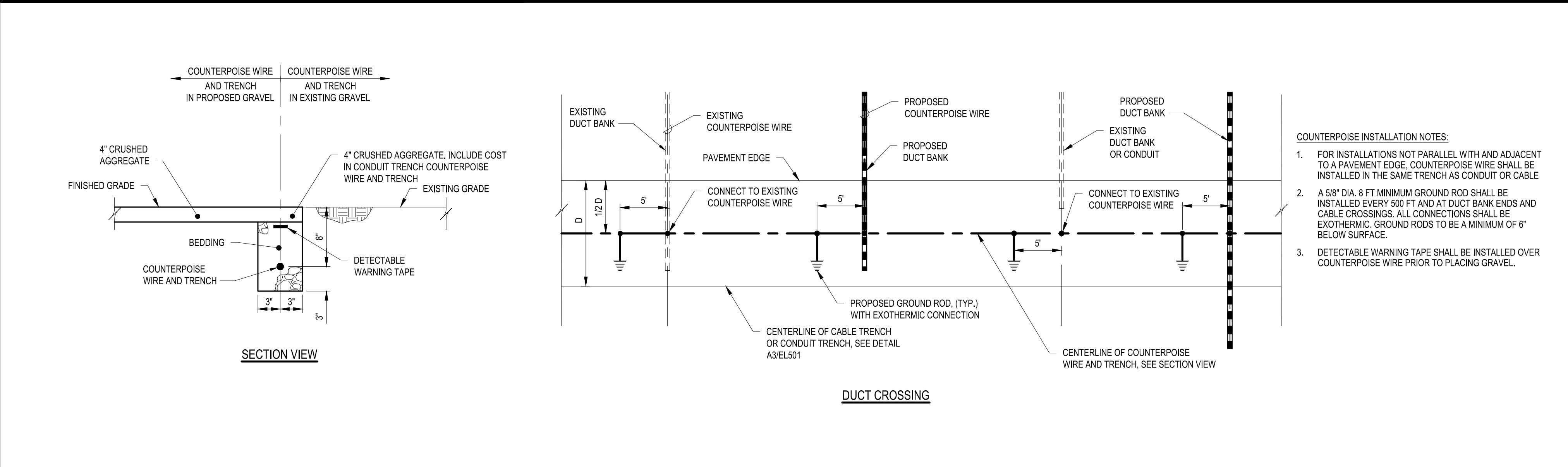
EL501
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- NOTES:
- A. ANGLE CONDUIT ENTRY INTO SIDES OF PULLBOX DOWNWARDS TO PREVENT WATER ENTRY INTO CONDUITS.
 - B. PROVIDE STONE AND GRAVEL BASE 24" DEEP UNDER PULLBOX.
 - C. SUBMIT SHOP DRAWINGS FOR REVIEW AND APPROVAL.
 - D. SET TOP PORTION OF PULLBOX ASSEMBLY APPROXIMATELY 2" ABOVE FINISHED GRADE.
 - E. STRUCTURE IS TO BE AIRCRAFT RATED.

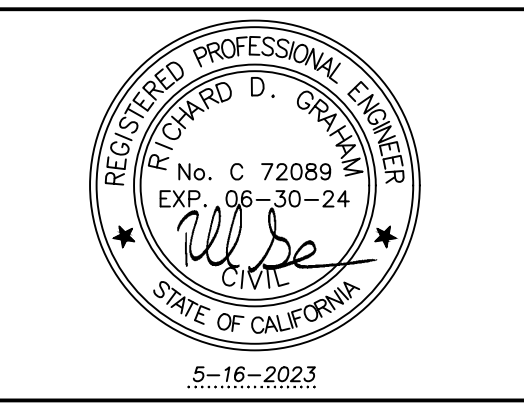
B1 ELECTRICAL STRUCTURE MODIFICATION DETAIL
SCALE: NOT TO SCALE



A1 COUNTERPOISE INSTALLATION DETAIL
SCALE: NOT TO SCALE



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IMPERIAL COUNTY AIRPORT
PAVEMENT REHABILITATION
(APMS 1 & 2) TAXIWAY B4/C5

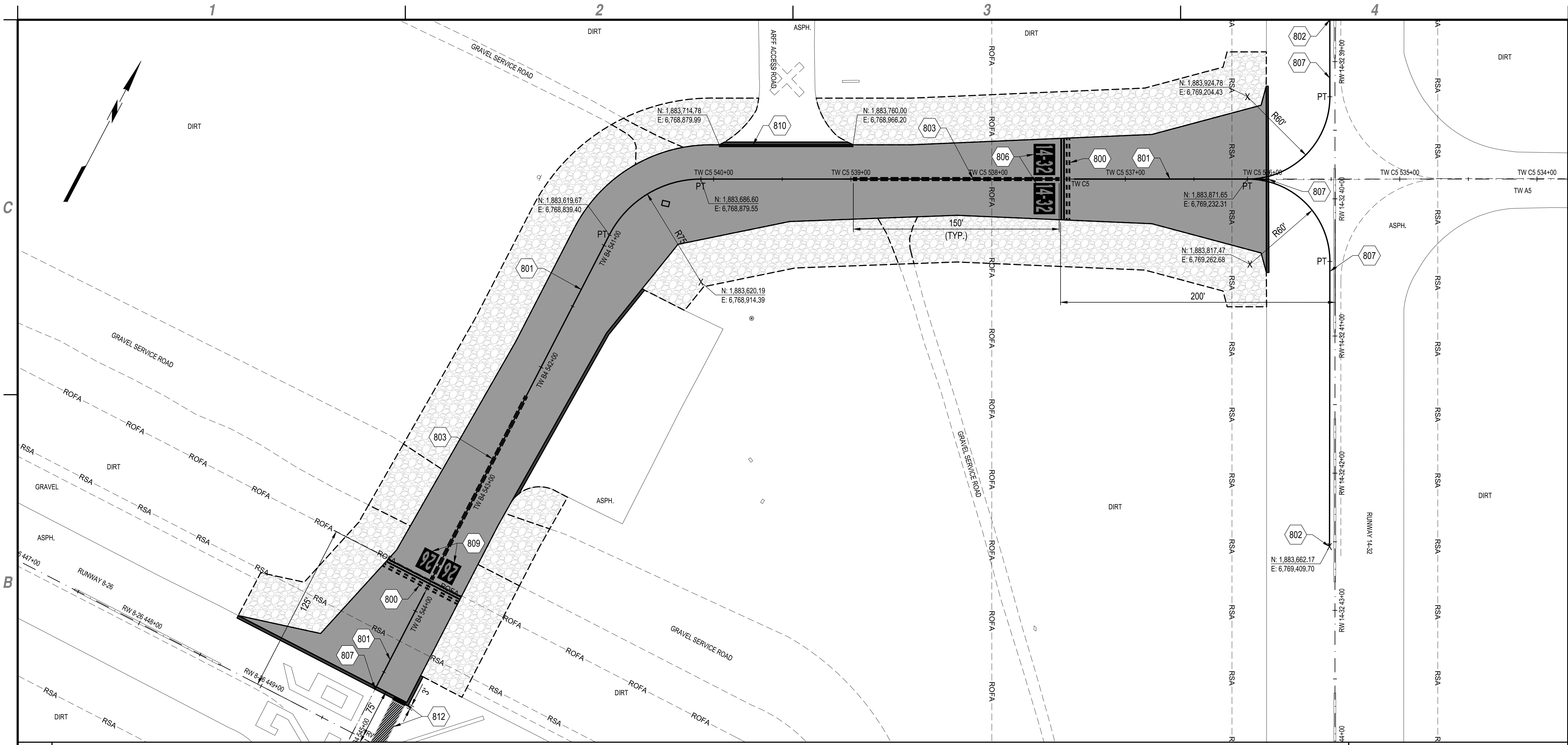
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PROJECT NO: K30002019		
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DRAWN BY: G.C. HAYDEN		
DESIGNED BY: M.E. BARR		
CHECKED BY: S.L. UNDERWOOD		

AIRFIELD LIGHTING AND SIGNAGE DETAILS

EL502
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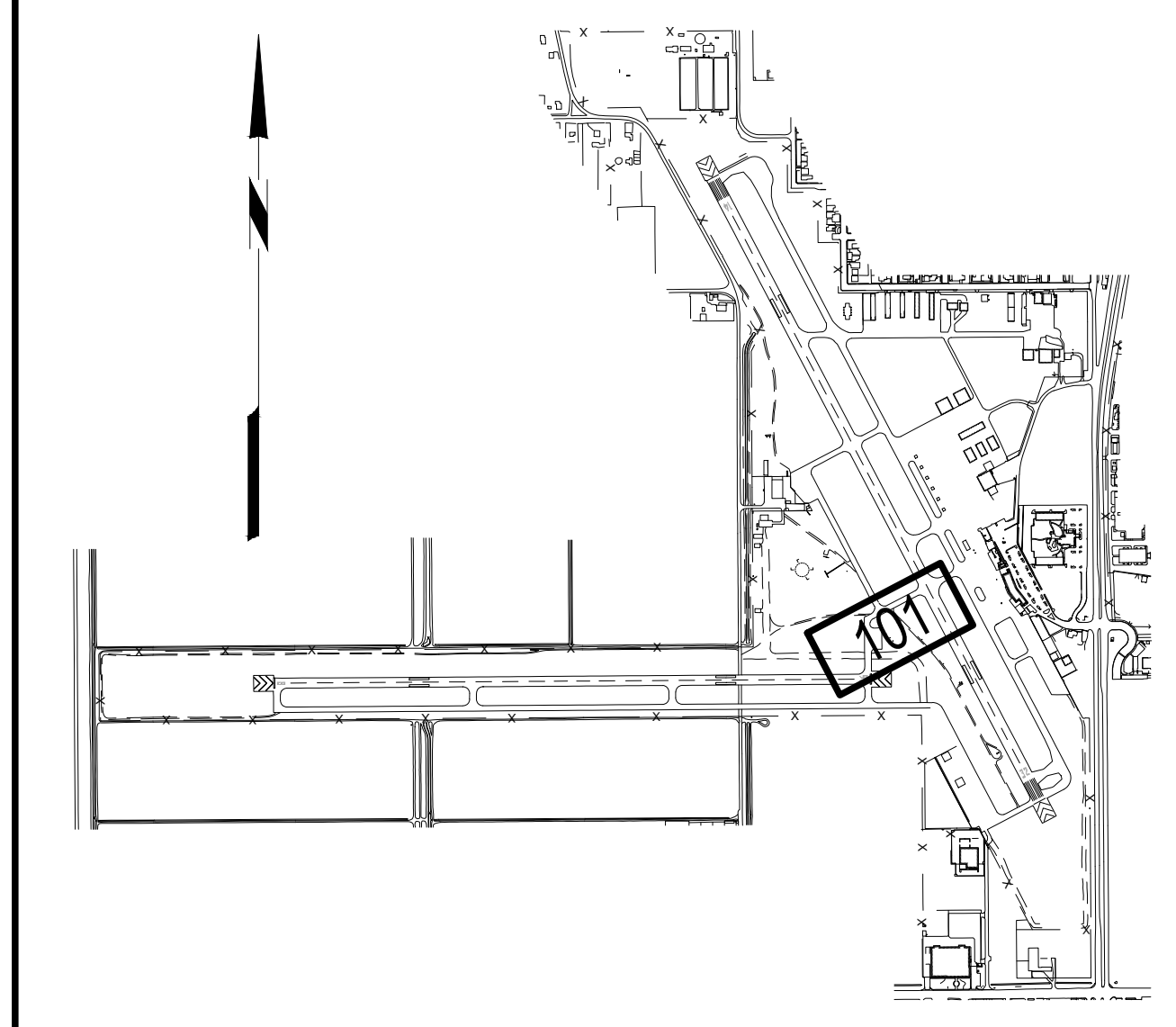
May 23, 2023 - 11:36AM
 F:\Project\K30 - Imperial County, CA\K30002019 - Pavement Rehabilitation (Design)\Design\CADD (Package 2)\Sheet Files\K30002019_XM1-SERIES.dwg



B1 MARKING PLAN
 SCALE: 1" = 40'

- 800. RUNWAY HOLDING POSITION MARKING, YELLOW WITH 6" WIDE BLACK BORDER, ITEM P-620. SEE DETAIL A3/XM501.
- 801. TAXIWAY CENTERLINE MARKING, 6" WIDE, YELLOW WITH 6" WIDE BLACK BORDER, ITEM P-620.
- 802. TAXIWAY CENTERLINE MARKING, 6" WIDE, YELLOW WITH 6" WIDE BLACK BORDER, ITEM P-620. SEE DETAIL A2/XM501 FOR RUNWAY CENTERLINE MARKING SEPARATION.
- 803. ENHANCED TAXIWAY CENTERLINE MARKING, YELLOW WITH BLACK BORDER, ITEM P-620. SEE DETAIL B1/XM501.
- 806. SURFACE PAINTED HOLDING POSITION SIGN 14-32, ITEM P-620. SEE DETAIL A1/XM502.
- 807. MEET EXISTING TAXIWAY CENTERLINE MARKING.
- 809. SURFACE PAINTED HOLDING POSITION SIGN 26, ITEM P-620. SEE DETAIL A1/XM502.
- 810. CONTINUOUS TAXIWAY EDGE MARKING, YELLOW WITH REFLECTIVE MEDIA, WITH 6" WIDE BLACK BORDER, ITEM P-620. SEE DETAIL A1/XM501.
- 812. PAINT NEW THRESHOLD BAR MARKING, 3' LONG. RE-PAINT EXISTING RUNWAY THRESHOLD BAR MARKING, 75' LONG. THRESHOLD MARKING IS 10' WIDE, WHITE WITH REFLECTIVE MEDIA, AND 6" WIDE BLACK BORDER, ITEM P-620

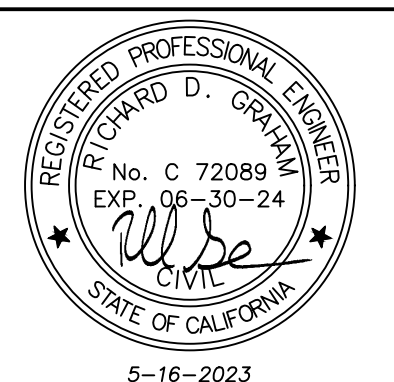
A1 KEYED NOTES
 SCALE: NOT TO SCALE



A4 KEY MAP
 SCALE: NOT TO SCALE



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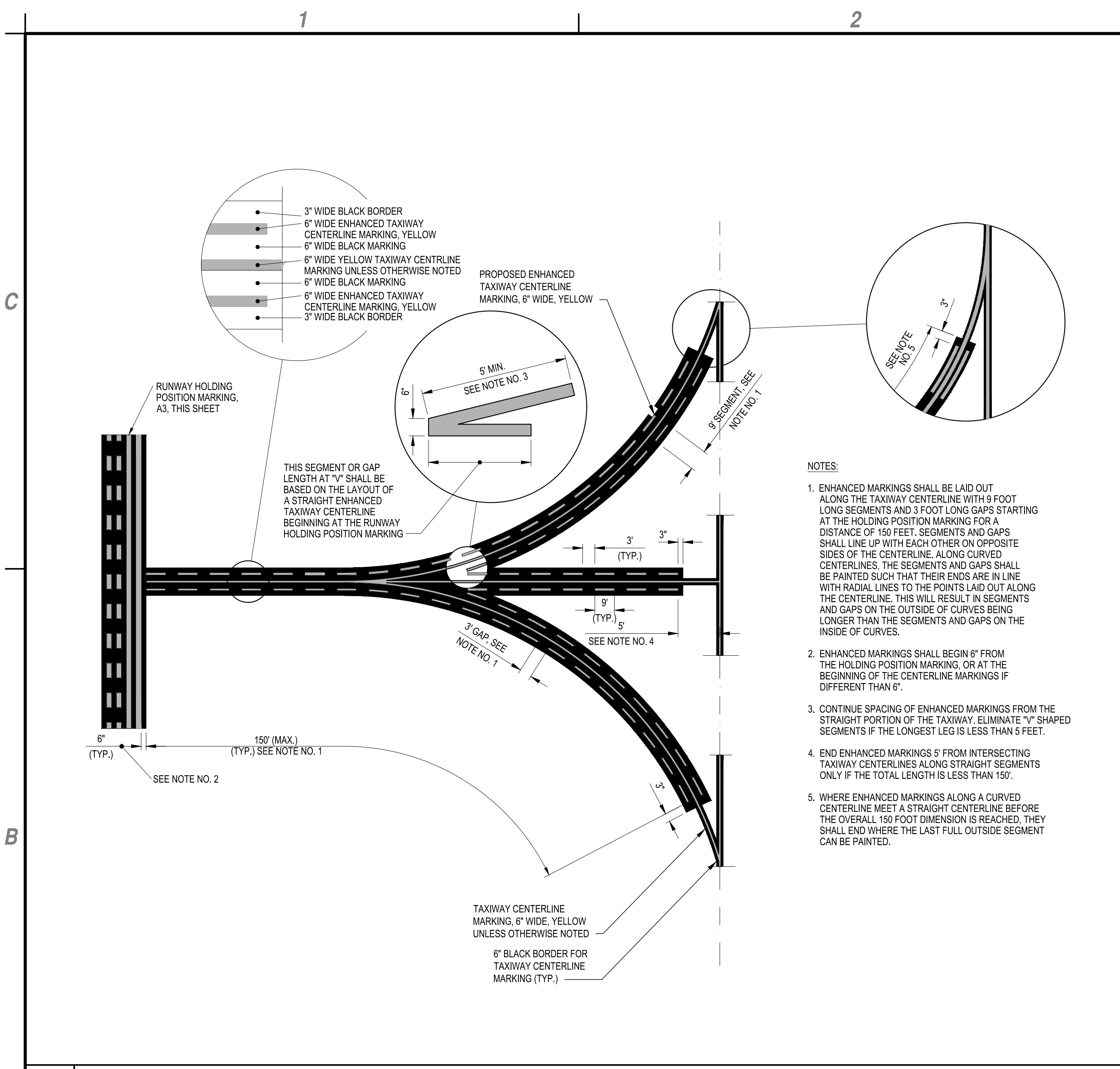
IMPERIAL COUNTY AIRPORT
**PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5**

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DATE: MAY 2023		
DRAWN BY: G.C. HAYDEN		
DESIGNED BY: M.E. BARR		
CHECKED BY: S.L. UNDERWOOD		

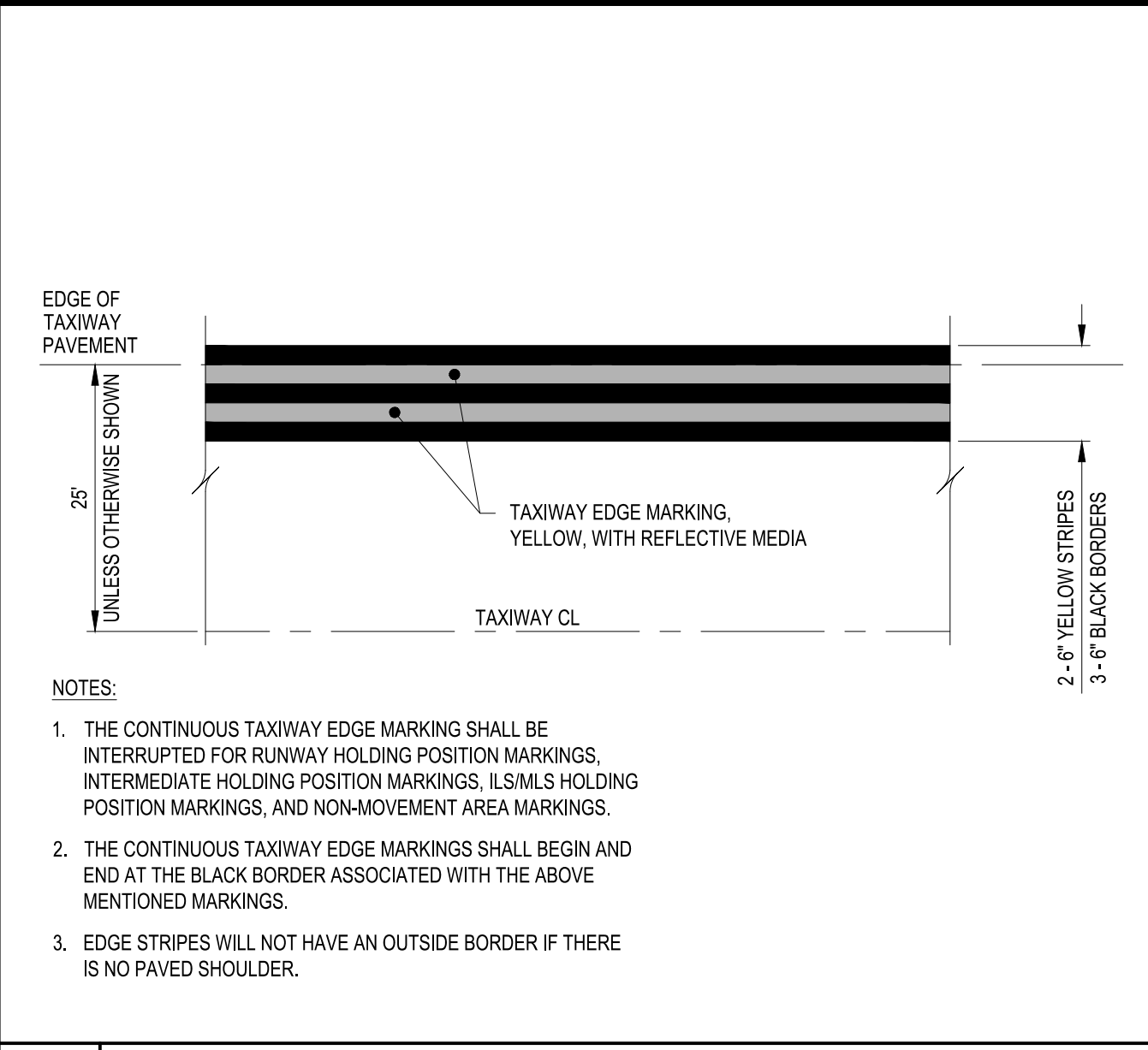
MARKING PLAN

XM101
 19 of 21

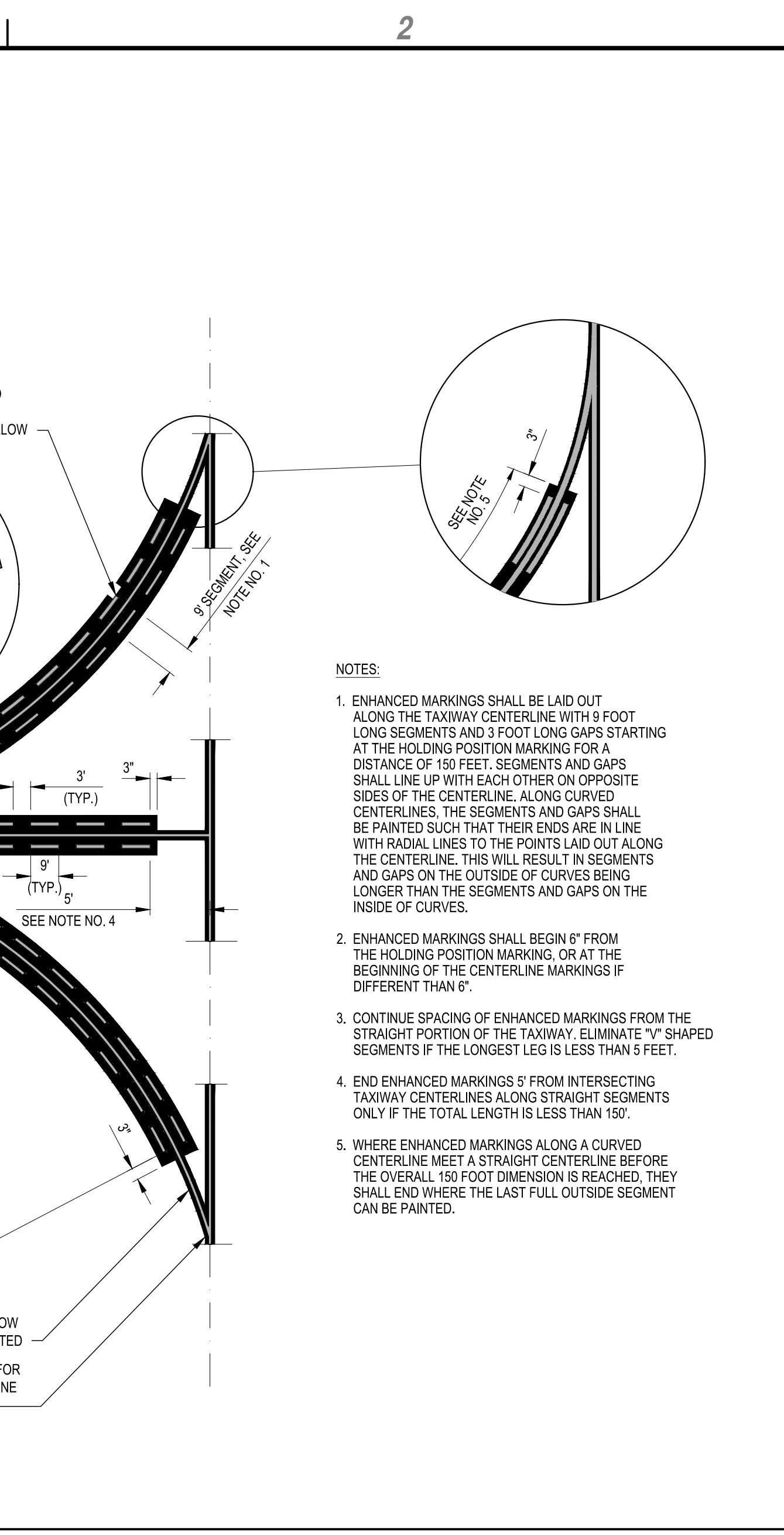
May 23, 2023 - 11:35AM
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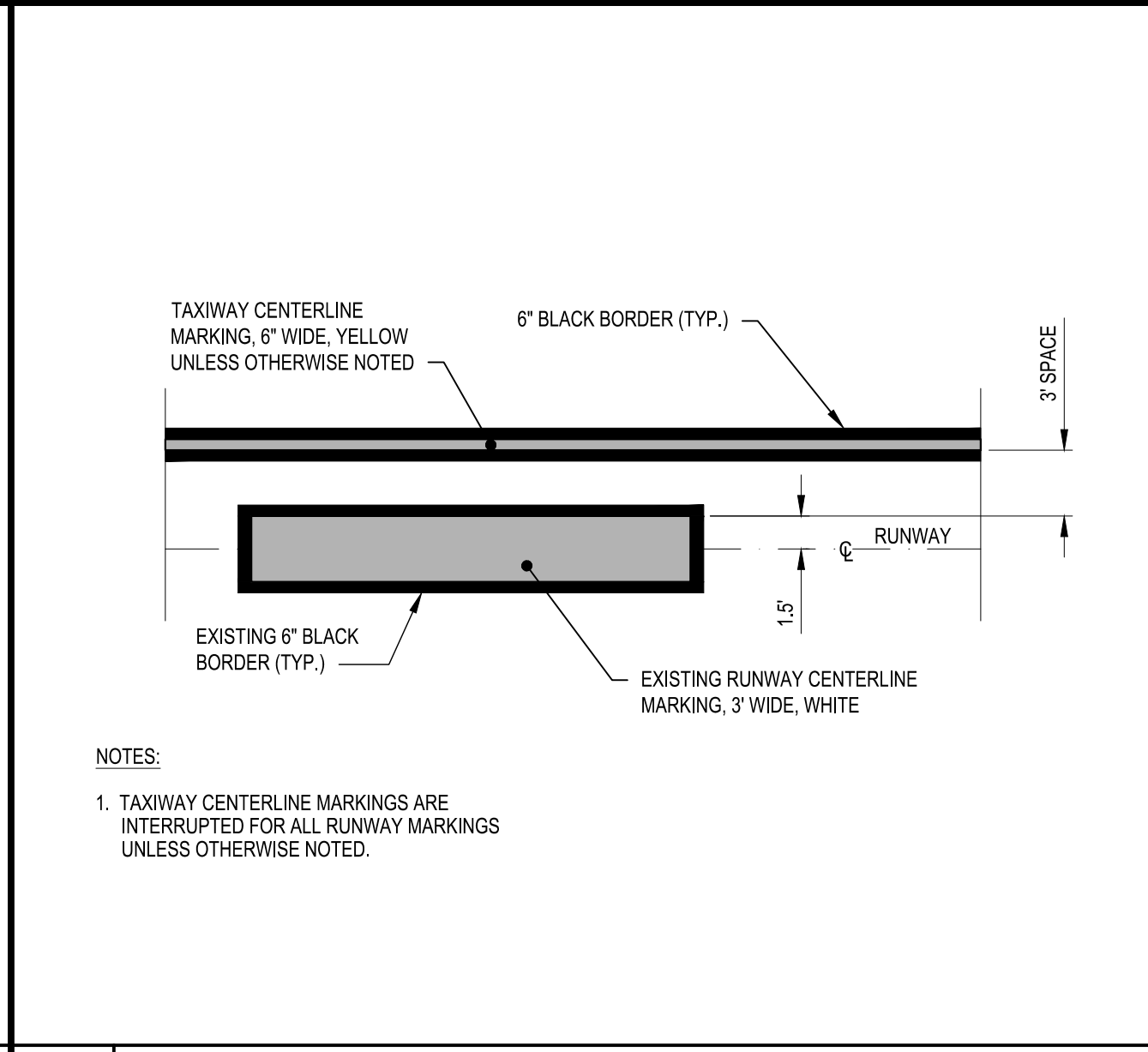
B1 ENHANCED TAXIWAY CENTERLINE MARKING DETAIL
SCALE: NOT TO SCALE



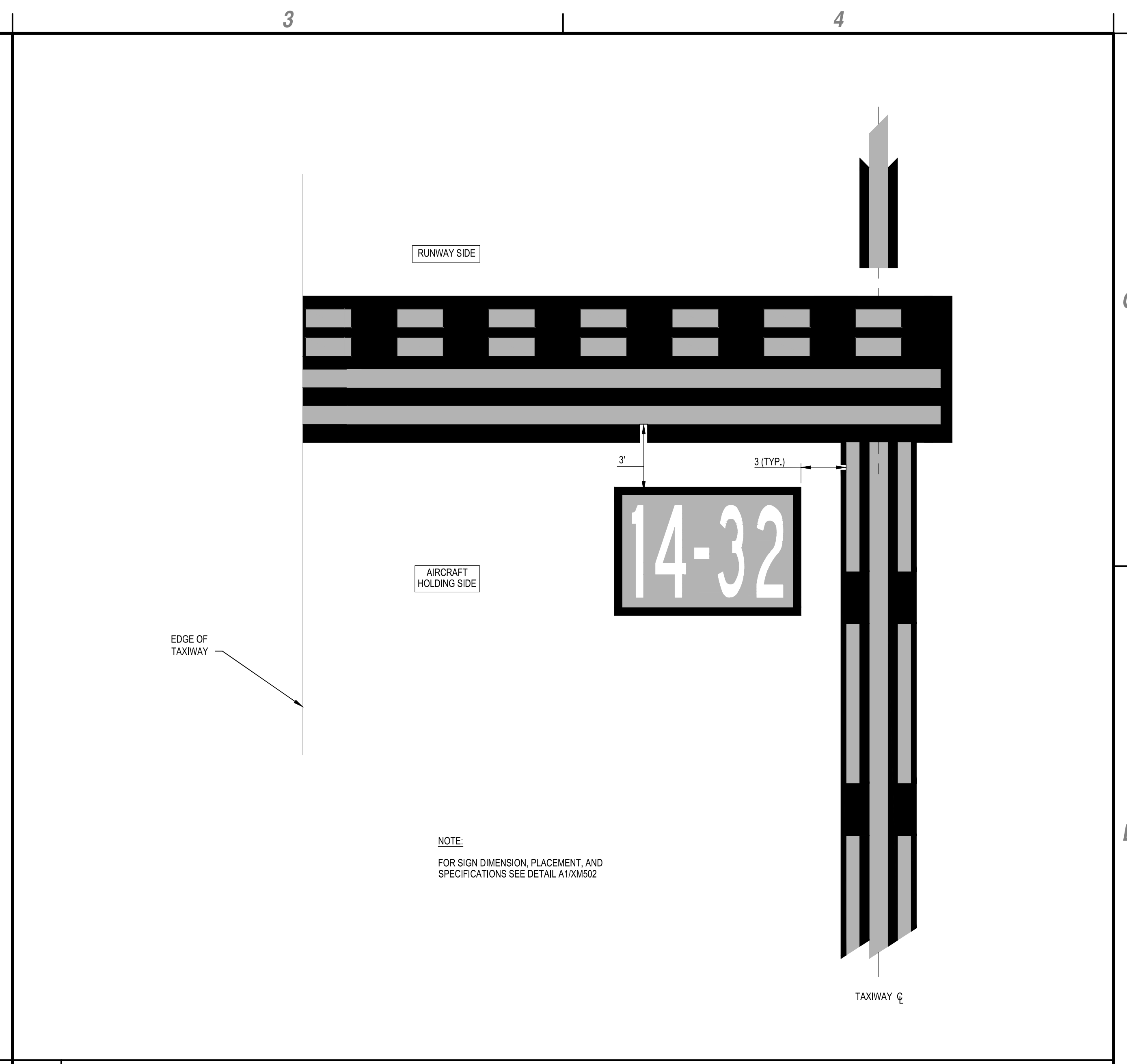
A1 CONTINUOUS TAXIWAY EDGE MARKING DETAIL
SCALE: NOT TO SCALE



A2 TW CENTERLINE MARKING AND SEPARATION DETAIL
SCALE: NOT TO SCALE



A3 RUNWAY HOLDING POSITION MARKING DETAIL
SCALE: NOT TO SCALE



B3 SURFACE PAINTED HOLDING POSITION PLACEMENT DETAIL
SCALE: NOT TO SCALE

C&S Engineers, Inc.
 2355 Northside Drive, Suite 350
 San Diego, California 92108
 Phone: 619-296-9373
 Fax: 619-296-5683
 www.cscos.com

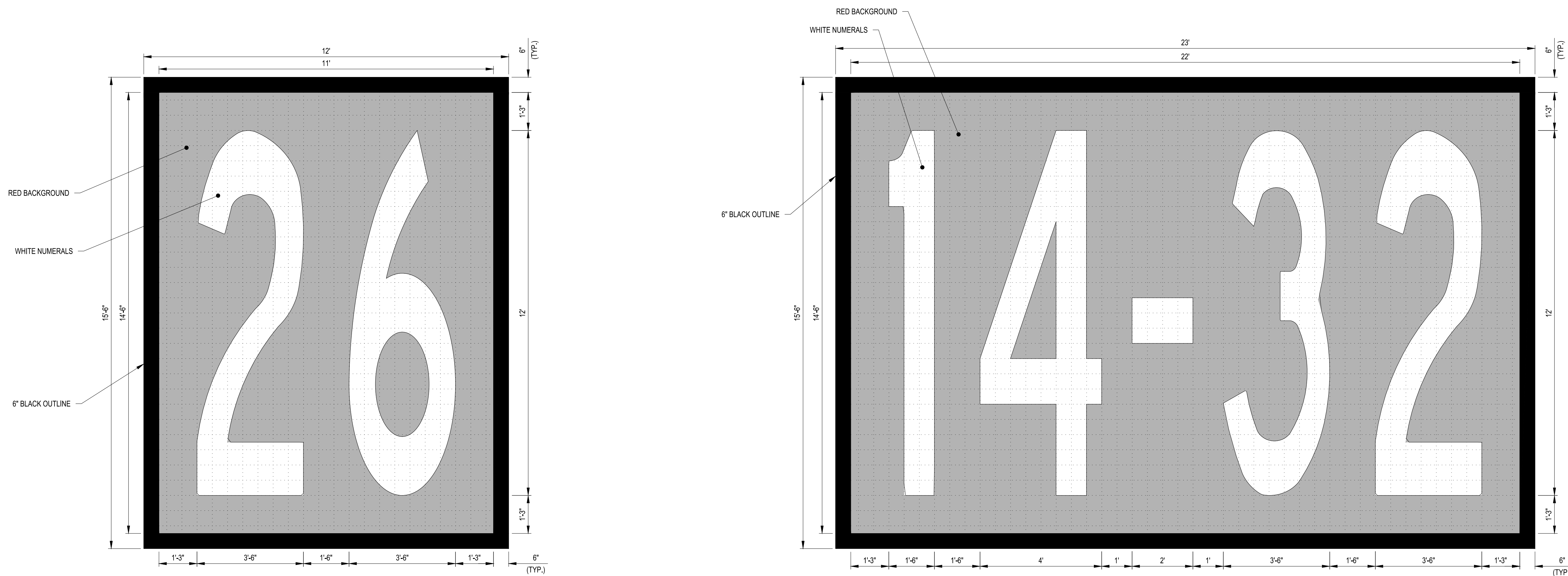
IMPERIAL COUNTY AIRPORT

**PAVEMENT REHABILITATION
(APMS 1 & 2) TAXIWAY B4/C5**

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO: K30020219		
DATE: MAY 2023		
DRAWN BY: G.C. HAYDEN		
DESIGNED BY: M.E. BARR		
CHECKED BY: S.L. UNDERWOOD		

MARKING DETAILS

XM501
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NOTES:

- 1. LETTERS AND NUMBERS MUST BE PROPORTIONAL TO THE HEIGHT SHOWN IN ORDER TO CONFORM TO SPECIFIED FONT TYPE. REFERENCE AC 150/5340-1M APPENDIX 1, LATEST EDITION.
- 2. ALL SURFACE PAINTED HOLDING POSITION SIGN INSCRIPTIONS ARE WHITE WITH GLASS BEADS, AS SPECIFIED. THE BACKGROUND IS RED PAINT WITH GLASS BEADS.
- 3. ALL SURFACE PAINTED HOLDING POSITION SIGN INSCRIPTIONS ARE TO BE PAINTED ON A RED BACKGROUND RECTANGULAR IN SHAPE AND EXTENDING BEYOND THE INSCRIPTION ON ALL SIDES AS SHOWN.
- 4. ALL SURFACE PAINTED HOLDING POSITION SIGNS ARE TO HAVE A BLACK BORDER 6 INCHES IN WIDTH, AS SPECIFIED.
- 5. SIGN LOCATION IS AT A MINIMUM OF 2 FEET AND A MAXIMUM OF 4 FEET FROM STOPBAR ON AIRCRAFT HOLDING SIDE, MEASURED FROM TOP EDGE OF SIGN BORDER. HORIZONTAL SIGN PLACEMENT IS ON THE LEFT SIDE OF TAXIWAY CENTERLINE AT A DISTANCE OF 3 FEET MINIMUM TO 10 FEET MAXIMUM MEASURED FROM THE CENTER OF TAXIWAY CENTERLINE TO THE RIGHT OUTERMOST EDGE OF SIGN. IN INSTANCES WHERE TAXIWAY CENTERLINE IS NOT PERPENDICULAR TO STOPBAR, THE CENTERLINE MEASUREMENT IS TAKEN TO THE CORNER OF SIGN BORDER NEAREST CENTERLINE. SEE DETAIL B3/XM501 FOR SAMPLE OF LAYOUT.
- 6. GRIDS ARE SHOWN FOR LAYOUT PURPOSES ONLY.
- 7. EACH GRID LINE REPRESENTS 6 INCHES.
- 8. CONTRACTOR SHALL CREATE A TEMPLATE FOR EACH INSCRIPTION REQUIRED.
- 9. TEMPLATES SHALL BE TURNED OVER TO THE OWNER UPON COMPLETION OF WORK.

SIGN 26

SIGN 14-32

A1 SURFACE PAINTED HOLDING POSITION SIGNS DETAILS

SCALE: NOT TO SCALE

May 23, 2023 - 11:35AM F:\Project\K30 - Imperial County, CA\K30002019 - Pavement Rehabilitation (Design)\CADD (Package 2)\Sheet Files\K30002019_XM-SERIES.dwg



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IMPERIAL CALIFORNIA
 IMPERIAL COUNTY AIRPORT
 PAVEMENT REHABILITATION
 (APMS 1 & 2) TAXIWAY B4/C5

MARK	DATE	DESCRIPTION
REVISIONS		
PROJECT NO:	K30002019	
DATE:	MAY 2023	
DRAWN BY:	G.C. HAYDEN	
DESIGNED BY:	M.E. BARR	
CHECKED BY:	S.L. UNDERWOOD	

MARKING DETAILS

XM502
 21 of 21